

SHOP TIPS

VOL. 13, NO. 10

NOVEMBER-DECEMBER, 1975

Motorcraft 



THIS MONTH
· EMERGENCY TOWING
· HOISTING
· STARTING

"TEST FOR SALES"

This "Free Inspection Tag" will assist you in recognizing a potential battery sale . . . and show your customer you are interested in his needs!

Now is the time to boost Motorcraft Battery sales. Take advantage of the many selling opportunities that exist when the "Test and Sell" concept is utilized. Use these "Free Battery Inspection" Tags (BD-2180) to pinpoint present or future battery sales . . . Now! These "Test for Sales" Tags are available free of charge from your Motorcraft Battery Supplier.

Dear Customer,

YOUR BATTERY HAS BEEN GIVEN A

FREE inspection which indicates

Battery Size _____

Condition of Case _____

Signs of Abuse _____

Cracks or Distortions _____

Water Level _____ Water Added _____

Cables:

Size _____

Condition _____

Terminals _____

State of Charge (Should read 1.250 to 1.270)

CELL 1	CELL 2	CELL 3	CELL 4	CELL 5	CELL 6
--------	--------	--------	--------	--------	--------


BATTERY IS OK

BATTERY NEEDS RECHARGING

BATTERY SHOULD BE REPLACED

Mr. Car Owner,

Your battery should be tested at least twice a year. The top of the battery, posts and cable terminals should be cleaned at that time.

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MOTORCRAFT PREMIUM HEAVY DUTY BATTERIES GIVE YOU MORE CRANKING TURNS, HIGHER CRANKING SPEED, AND LONGER CRANKING TIME THAN CONVENTIONAL RUBBER CASE BATTERIES. BUILT TO DELIVER MAXIMUM POWER! BUILT TO DELIVER MAXIMUM RESERVE CAPACITY!

MOTORCRAFT'S "GOLD LINE" IS THE "PEACE-OF-MIND" LINE!

MOTORCRAFT PREMIUM BATTERIES FEATURE: ORIGINAL EQUIPMENT QUALITY AND CAPACITY. MOST HAVE A LIGHT WEIGHT POLYPROPYLENE CASE FOR HIGH IMPACT RESISTANCE, POSITIVE SEALING AND FOR EASE OF CLEANING. SHORT INTERCELL CONNECTORS - FOR LESS ELECTRICAL RESISTANCE AND HIGH STARTING VOLTAGE. VACUUM DRIED PLATES - TO ASSURE MAXIMUM POWER AND FRESHNESS.

MOTORCRAFT PREMIUM BATTERIES OFFER THE FEATURES THAT PERFORM . . . THE BENEFITS THAT LAST!



MOTORCRAFT STANDARD BATTERIES FEATURE: CONTROLLED QUALITY WITH MANY PREMIUM FEATURES. ONE PIECE COVER - TO RETARD CORROSION, LEAKAGE AND POWER DRAIN. VACUUM DRIED PLATES - FOR MAXIMUM POWER AND FRESHNESS. HARD RUBBER CASE - TO RESIST HEAT AND IMPACT. SHORT INTER-CELL CONNECTORS - FOR LESS ELECTRICAL RESISTANCE AND HIGH STARTING VOLTAGE.

MOTORCRAFT STANDARD BATTERIES . . . PRICED RIGHT . . . BUILT TO PERFORM!

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Bd 2180

EMERGENCY TOWING and STARTING TIPS ... PLUS HOISTING PROCEDURES

FOR 1975-76 FORD MOTOR COMPANY CARS

When the Phone Rings . . .

You have to be prepared for all sorts of responses: to friends, salesmen, question-askers . . . and people with real problems, a disabled car. Probably nothing is more irritating than a vehicle that won't run . . . especially when the owner is in a hurry. And it can be frustrating to you during peak work periods. But, as they say, emergency service goes with the territory. If you handle them satisfactorily, however, these calls for help can lead to lots of repeat business.

For openers, devise a scheduling system with your employees to handle emergencies. And be sure you have the proper equipment before committing to an owner that you can handle the problem. These might include a good towing rig, towing dolly, scotch blocks, tow bar and sling. You should also make a set of wood block adapters and consider the use of a two-way radio and a gas-powered generator/starter.

When the customer calls, try to handle the situation as courteously as possible. Getting off on the right foot is very important. Ask the necessary questions to determine the exact nature of the problem. A "flat tire" or "out of gas" problem is easily solved. More serious breakdowns may require extra tools or parts to correct "on the spot" . . . or you may have to tow the vehicle back to your shop, depending on what you find out from the customer.

If you decide to tow . . . be sure you hook-up correctly and exercise care not to damage the customer's car. To assist you, this article illustrates the recommended towing procedures for 1975-76 Ford-built cars. We've also included hoisting and starting tips that you should find helpful. Why not review these details NOW and make notes on any special equipment or materials needed. Then, when the phone rings, you'll be prepared to handle any emergency in a professional manner.

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Be sure to file this and future issues for ready reference. If you have any suggestions for articles that you would like to see included in this publication, please write to: Ford Parts and Service Division, Parklane Tower West, Publications Section, One Parklane Blvd., Dearborn, Michigan 48126.

The information in this publication was gathered from materials released by various technical departments of the Ford Motor Company, the Ford Parts and Service Division of the Ford Motor Company, as well as other vehicle and parts manufacturers. The descriptions and specifications contained in this issue were in effect at the time it was approved for printing. Our policy is one of continuous improvement and we reserve the right to change specifications or design without notice and without incurring obligation.

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Livonia, Michigan

EMERGENCY TOWING, HOISTING and STARTING

GENERAL TOWING INSTRUCTIONS

Before attempting to tow a vehicle, the tow operator should check to see that the parking brake is released and determine if the transmission and rear axle are in proper working order. If the rear axle is damaged or has failed, the rear wheels **MUST** be raised off the pavement and the car towed with the front wheels in the straight ahead position . . . or the front wheels supported on a wheeled dolly (Figure 1). If the transmission is damaged or inoperative, the driveshaft **MUST** be removed prior to towing the vehicle, or the rear wheels raised or supported on a wheeled dolly . . . depending on which is more convenient.

To tow a vehicle with both the steering column and the transmission locked, and no ignition key available, lift the vehicle from the rear or use a wheeled dolly under the rear wheels and tow from the front.

NOTE: Late model cars equipped with an automatic transmission (selector lever in PARK position and no ignition key) will have the rear wheels **LOCKED**. Therefore, the car must be towed with the rear wheels raised, or placed on a wheeled dolly.

If the front wheels are turned and locked, then the front wheels must be supported with a wheeled dolly and the car towed from the rear.

If the car is towed with the rear wheels on the roadway, NEVER EXCEED 30 mph or tow for distances greater than 15 miles. If it is necessary to exceed these limits, then the tow operator must tow the car with (1) the rear wheels off the ground (if possible), (2) with the driveshaft removed, or (3) with the rear wheels supported on a wheeled dolly.

PRECAUTIONS WHEN TOWING

NOTE: If your tow truck has an adjustable boom, make sure it's in proper position before lifting or towing (Figure 2).

1. Never tow over 30 mph (where permitted). The distance required to stop safely increases greatly with speed. Remember, you're driving **TWO** vehicles.
2. Never start or stop suddenly. Be alert and try to anticipate stops and turns. Panic stops not only may cause accidents, but also may damage the towed vehicle. Also remember that jerky driving can cause sheet metal and bumper damage.
3. Pass carefully . . . if you must pass. Your tow truck's length is increased by about 20 feet . . . meaning that you need at least 5 extra vehicle lengths for passing room.
4. Never allow passengers to ride in towed vehicles.
5. Turn the parking lights of the towed vehicle **ON** when towing at night.

CAUTION: If the towed vehicle is not trailering properly, or is unstable, stop **IMMEDIATELY** and determine the reason.

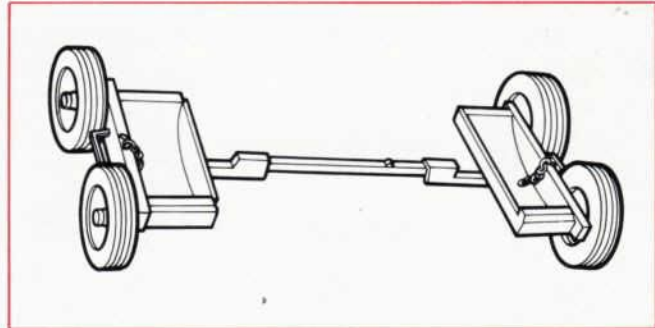


Figure 1 — Here is a typical and popular style of wheeled dolly that can assist the tow operator in several ways. Its use avoids removing the driveshaft in bad weather, and is extremely helpful when towing a car with no wheels, or complete wrecks.

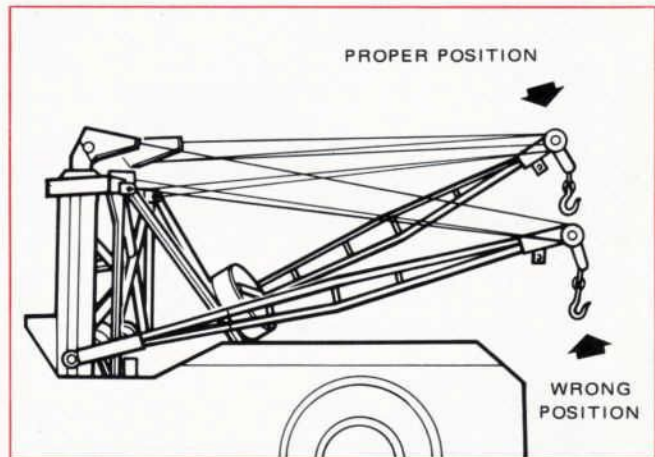


Figure 2 — If your tow truck has an adjustable boom, be sure it's in the proper position when lifting and towing vehicles.

TOWING SLINGS

To avoid metal-to-metal contact and possible damage to the chrome or lower body panels, a special wide-belt sling should be used to lift and tow **ALL** cars. When attaching towing slings, take care to avoid damage to license plate and frame. Vehicles with a front spoiler may need to have the spoiler removed from the vehicle before it's towed; either from the front or rear. For the Mustang II "Cobra", which has a front spoiler, the suggested service procedures are:

- The Cobra II should be towed from the rear whenever possible.
- If front end tow is required, the spoiler should be lifted with the hand, as the chain of the tow truck tightens, to insure that it bears on the unpainted side of the spoiler and does not crimp or drastically fold the spoiler.

NOTE: The 1975-76 Lincoln Continental (rear) and Rancho (rear) require a 4" x 4" block of wood to act as a spacer to avoid excessive stress on the lower body panels.

EMERGENCY TOWING, HOISTING and STARTING

TOWING SLING and TOW BAR INSTALLATION — FORD MOTOR COMPANY CARS . . . 1975 SHOWN, 1976 SIMILAR

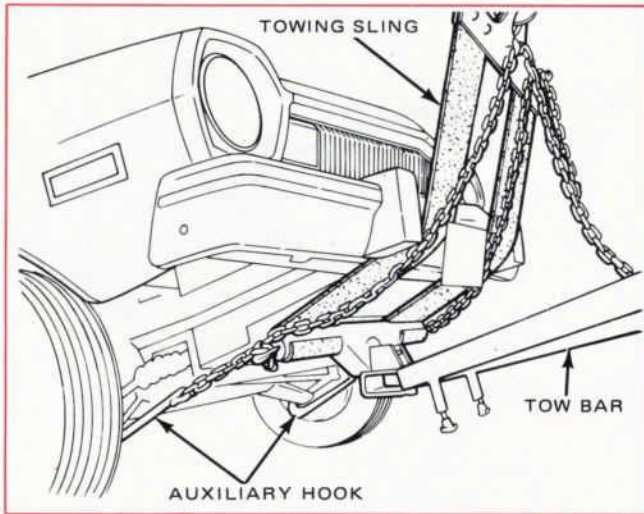


Figure 3 — Pinto/Bobcat — Front

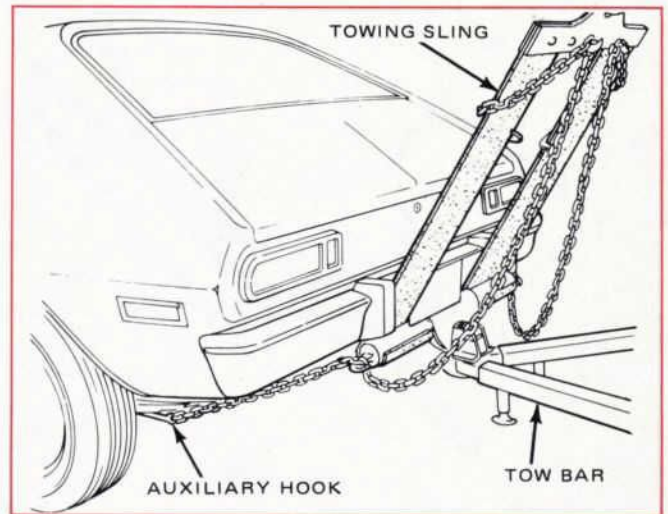


Figure 4 — Pinto/Bobcat — Rear

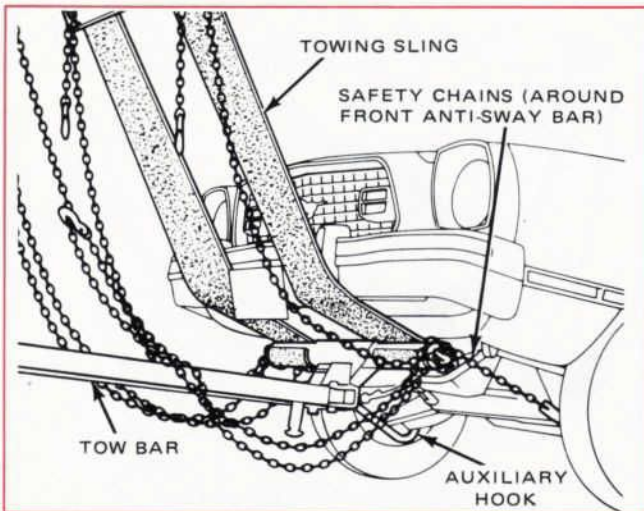


Figure 5 — Mustang II — Front*

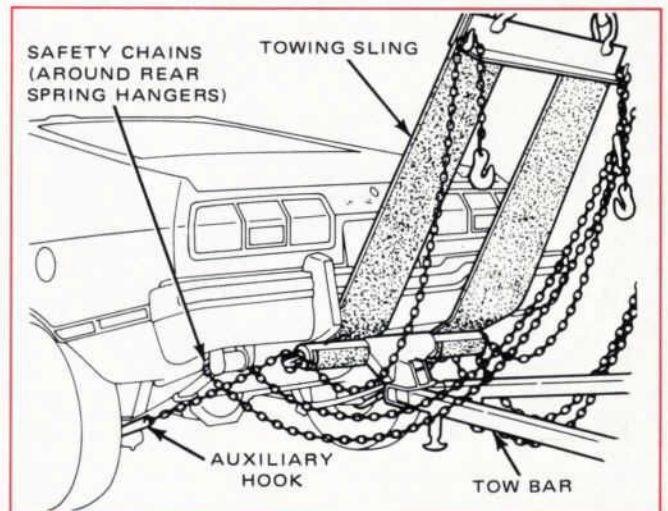


Figure 6 — Mustang II — Rear

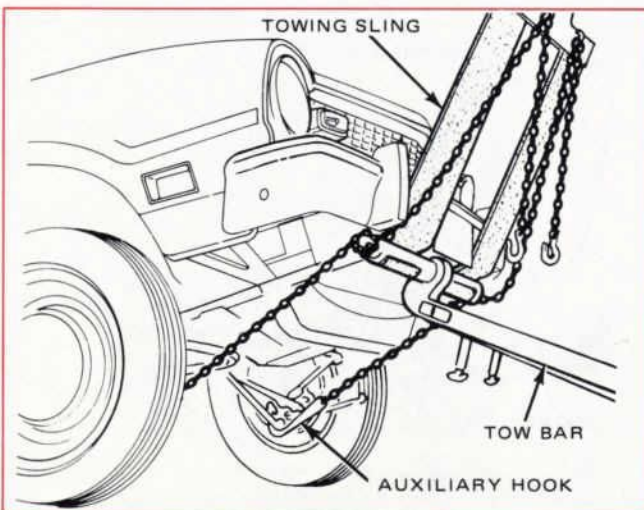


Figure 7 — Maverick/Comet — Front

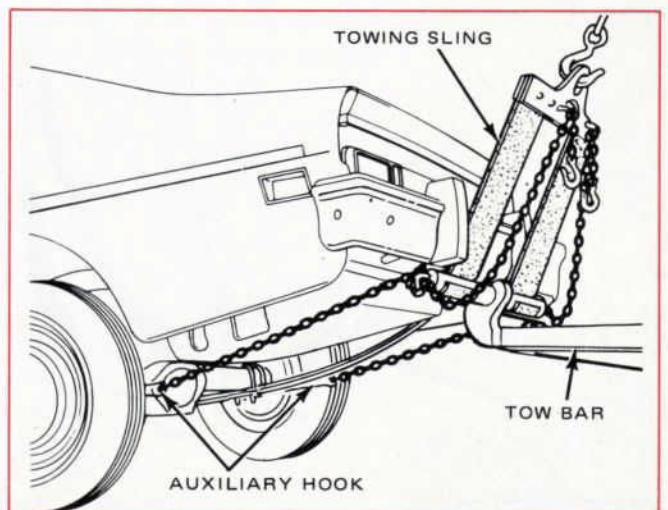


Figure 8 — Maverick/Comet — Rear

*Except Mustang II "Cobra" — see "Towing Slings"

EMERGENCY TOWING, HOISTING and STARTING

TOWING SLING and TOW BAR INSTALLATION — FORD MOTOR COMPANY CARS . . . 1975 SHOWN, 1976 SIMILAR

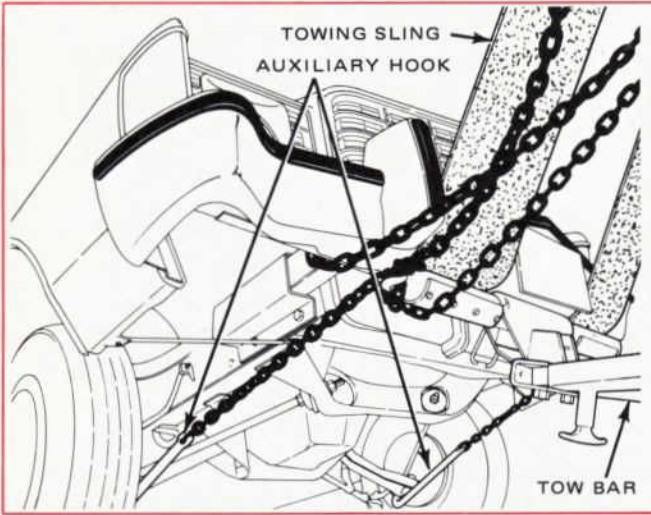


Figure 9 — Granada/Monarch — Front

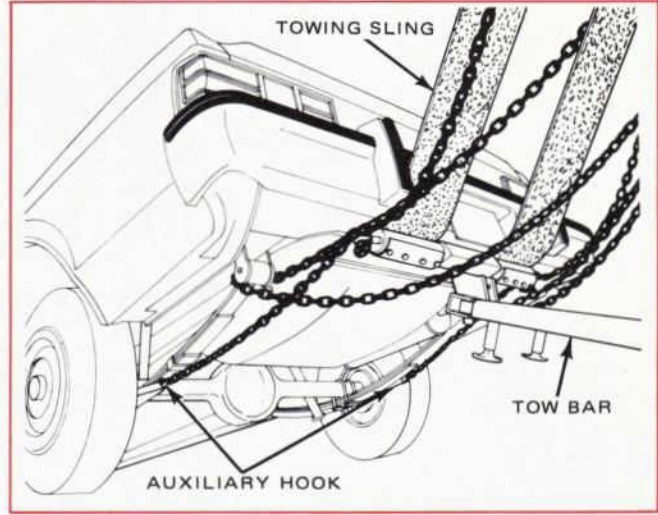


Figure 10 — Granada/Monarch — Rear

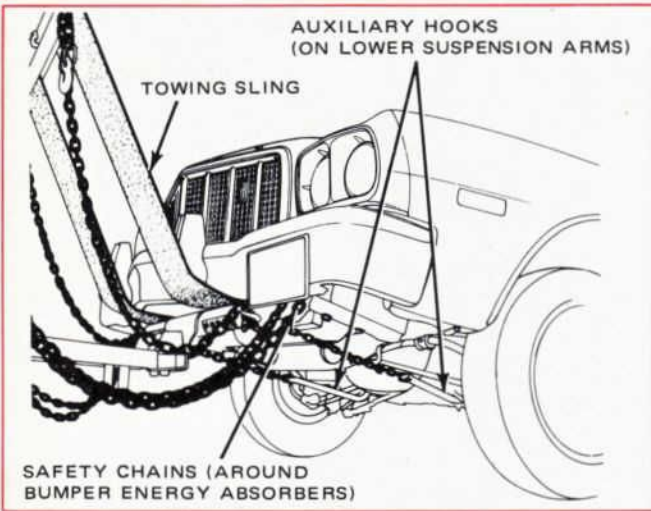


Figure 11 — Torino — Front

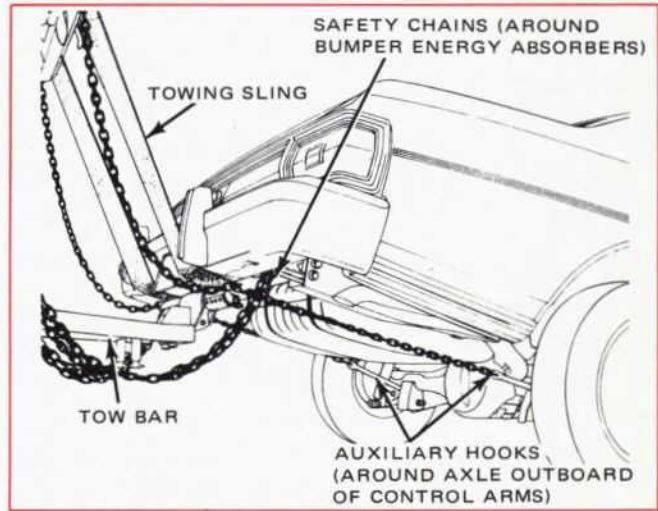


Figure 12 — Torino — Rear

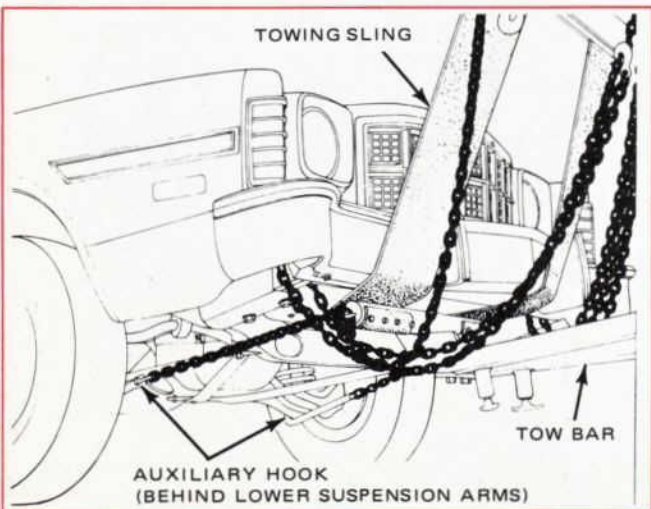


Figure 13 — Elite — Front

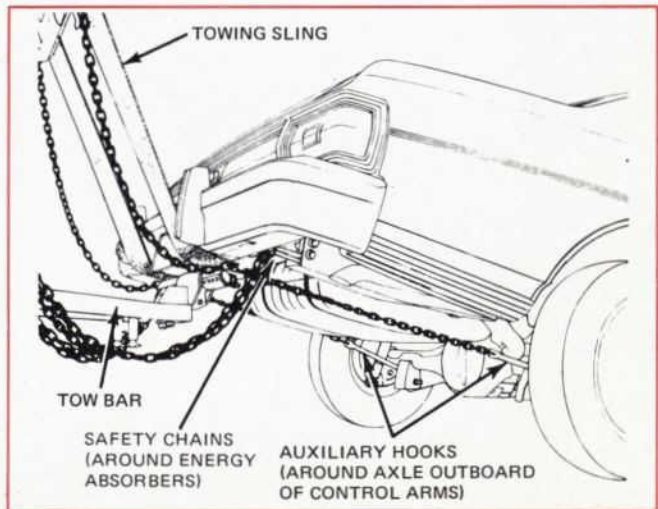


Figure 14 — Elite — Rear

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TOWING SLING and TOW BAR INSTALLATION — FORD MOTOR COMPANY CARS . . . 1975 SHOWN, 1976 SIMILAR

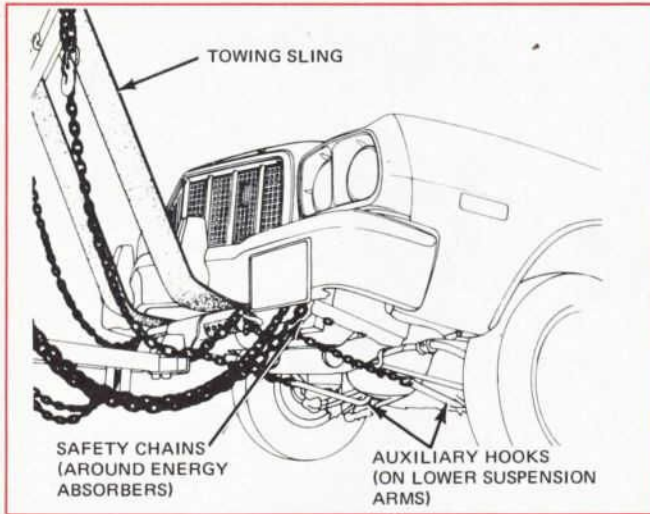


Figure 15 — Ranchero — Front

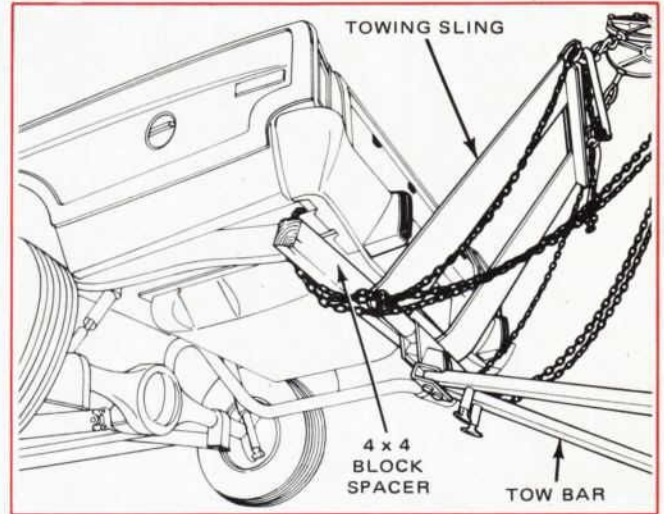


Figure 16 — Ranchero — Rear

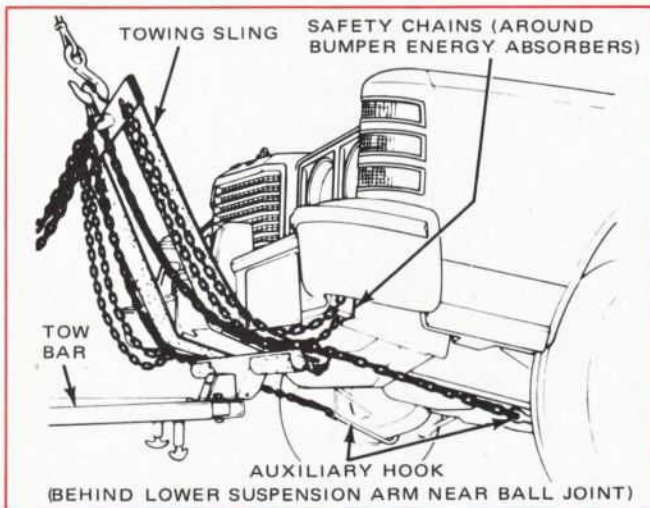


Figure 17 — Ford — Front

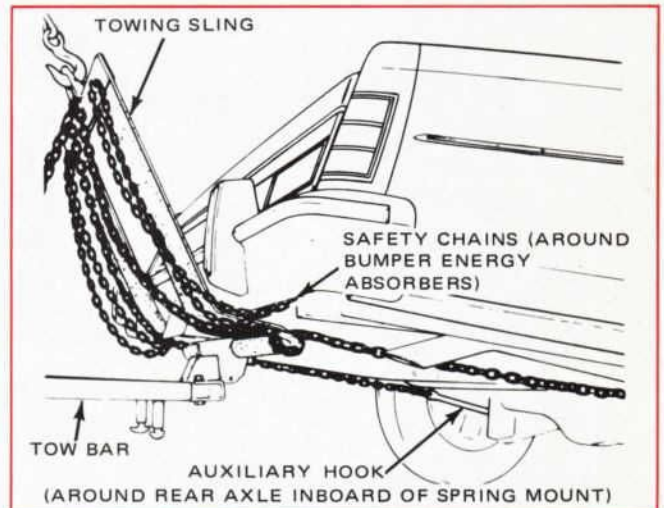


Figure 18 — Ford — Rear

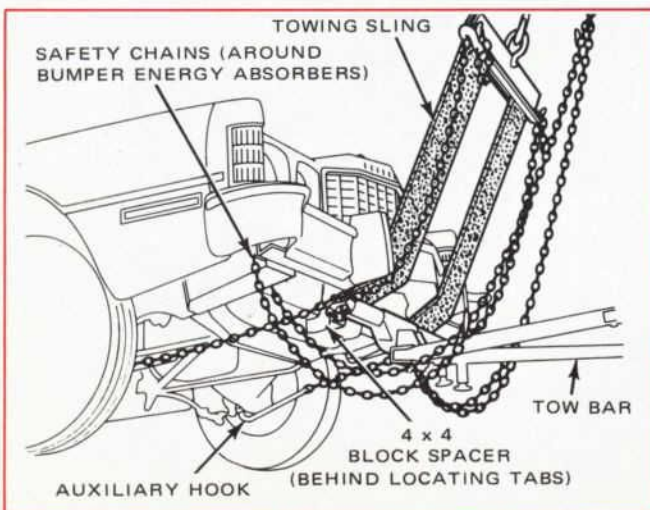


Figure 19 — Thunderbird — Front

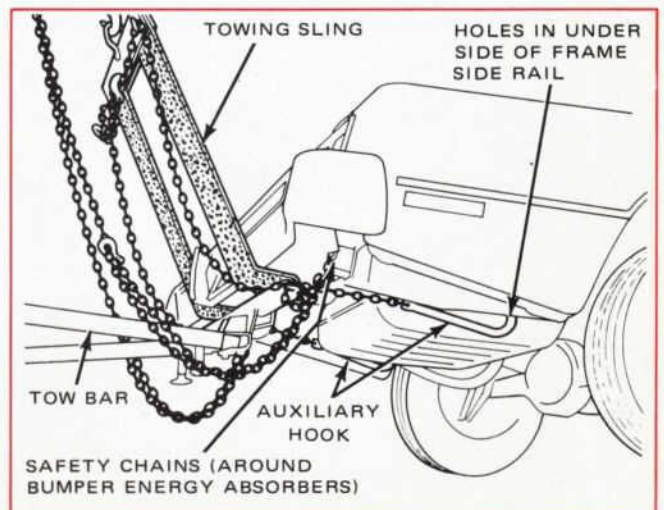


Figure 20 — Thunderbird — Rear

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Capture the Do-It

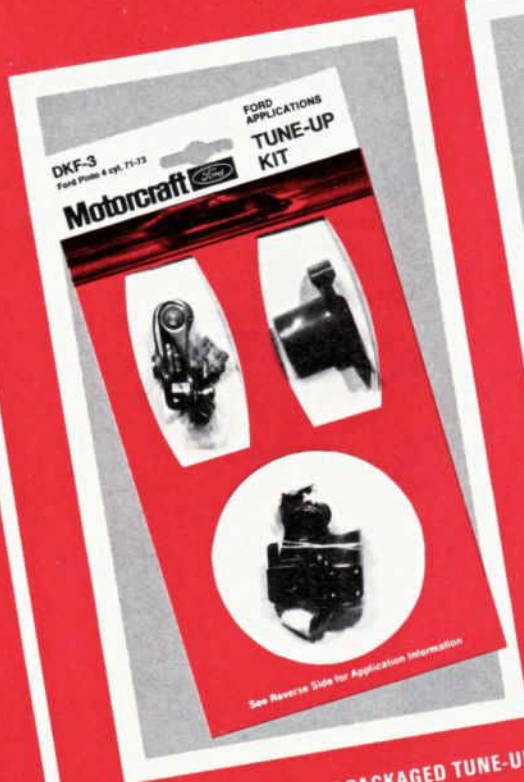
...With These *Fast-Moving* Products

THE DO-IT-YOURSELF MARKET OFFERS A RAPIDLY EXPANDING SALES OPPORTUNITY FOR YOU!

Every recent study indicates a growing number of car owners are performing a portion of their own maintenance and repair work. Many are involved in engine tune-ups, which require the purchase and installation of spark plugs and other electrical ignition parts.

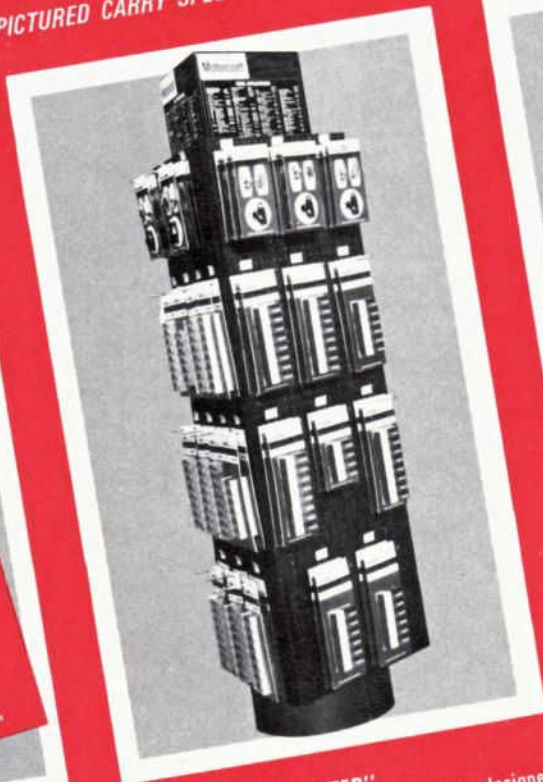
Offer your do-it-yourself customers fast-moving display packaged Autolite Spark Plugs and Motorcraft Electrical Parts, as well as popular Motorcraft Oil Filters, and cash in big on this growing market!

ALL OF THE ITEMS PICTURED CARRY SPECIFIC APPLICATION INFORMATION.



MOTORCRAFT DISPLAY PACKAGED TUNE-UP KITS

Points, condenser and rotor assortments . . . the fastest selling, most popular part numbers attractively packaged to catch the attention of the do-it-yourself market. Just seven part numbers cover 85% of Ford, GM and Chrysler applications.



"THE ATTENTION GETTER"

The "Attention Getter" display rack is designed specially to make it easier for you to stock, display and sell Motorcraft. This handsome six foot rotating display rack comes to you preassembled . . . and includes 36 safety hooks, 36 pressure-sensitive I.D. tags and an application chart. Available with a recommended assortment of Motorcraft Tune-Up Kits and display packaged Autolite Spark Plugs.



DISPLAY PACKAGED AUTOLITE SPARK PLUGS
These eye-appealing display packs are designed to offer you additional opportunities for increased sales and profits! It takes only eighteen part numbers to cover 85% of Ford, GM and Chrysler applications. Available in eight, six and four plug packages.

helps you -Yourself Market

Packaged for *Quick Self-Service Sales!*

INTRODUCING . . .
MOTORCRAFT DISPLAY PACKAGED
OIL FILTERS



PLUGS
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ed sales
umbers to
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MOTORCRAFT "SIPCON" IGNITION SETS

Twenty part numbers are now available in this new Motorcraft display packaged carton. These twenty fast-moving part numbers provide extensive coverage for Ford, GM and Chrysler applications. Merchandise this brilliant new "Sipcon" display packaged carton three different ways: hanging, book style or stacked. "Sipcon" tailored sets are original equipment on all Ford-built cars and trucks and meet original equipment specifications.



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This Motorcraft "first" means convenience and more potential business for you with everything you need for a professional tune-up together in one single package. Each Motorcraft Electrical Tune-Up Kit contains: Autolite Spark Plugs (4, 6 or 8), distributor point set, condenser, feeler gauge and cam lube, installation instructions.

Motorcraft Electrical Tune-Up Kits offer a broad application market with over 80% of all American cars plus many import cars—assuring you maximum coverage of the tune-up business at minimum inventory investment.



The same high quality Motorcraft Oil Filter with a bright new appearance. Features attractive new filter graphics and provides high product visibility in a sparkling display package. This new see-through package has sell appeal for everyone. Eight popular Motorcraft Display Packaged Oil Filters offer extensive coverage for Ford, GM and Chrysler applications.

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Contact Your Motorcraft Supplier Today!

EMERGENCY TOWING, HOISTING and STARTING

TOWING SLING and TOW BAR INSTALLATION — FORD MOTOR COMPANY CARS . . . 1975 SHOWN, 1976 SIMILAR

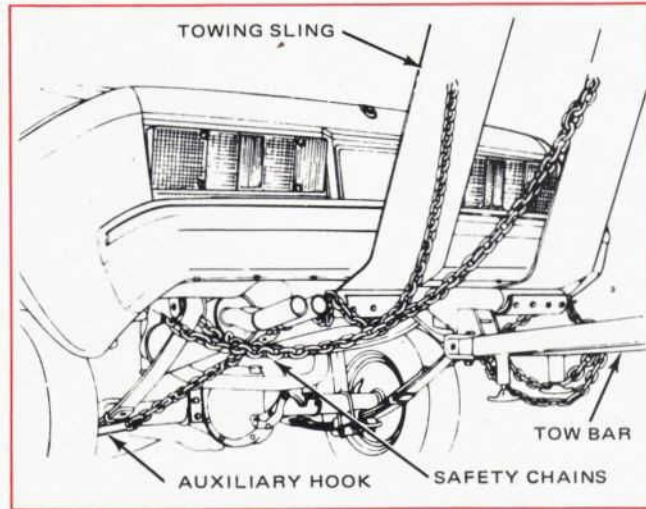


Figure 21 — Capri II — Rear

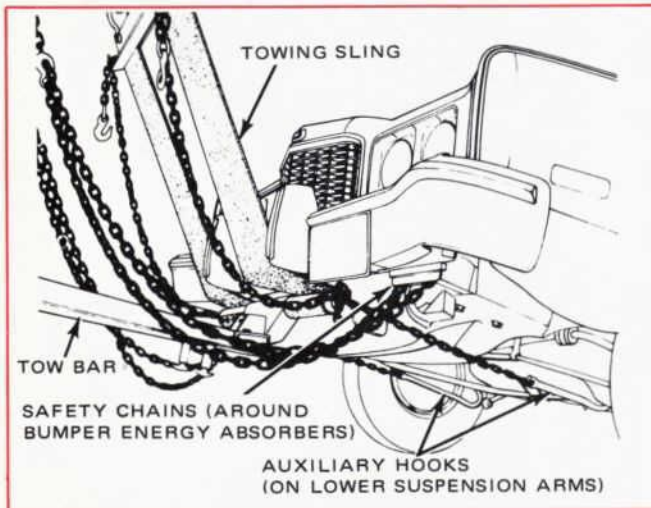


Figure 22 — Montego — Front

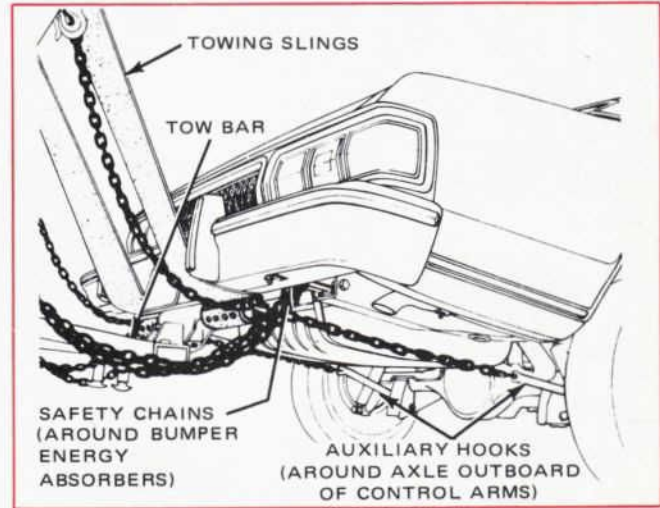


Figure 23 — Montego — Rear

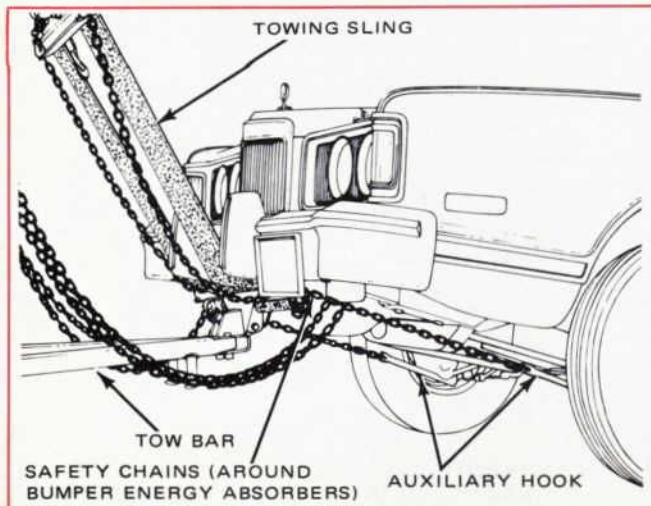


Figure 24 — Cougar — Front

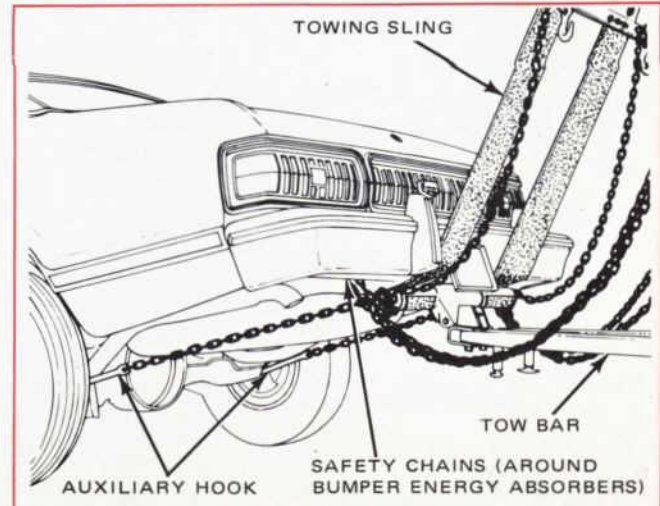


Figure 25 — Cougar — Rear

EMERGENCY TOWING, HOISTING and STARTING

TOWING SLING and TOW BAR INSTALLATION — FORD MOTOR COMPANY CARS . . . 1975 SHOWN, 1976 SIMILAR

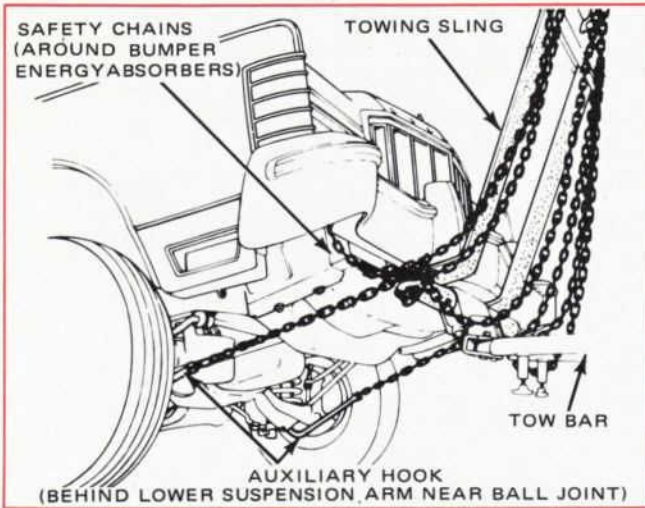


Figure 26 — Mercury — Front

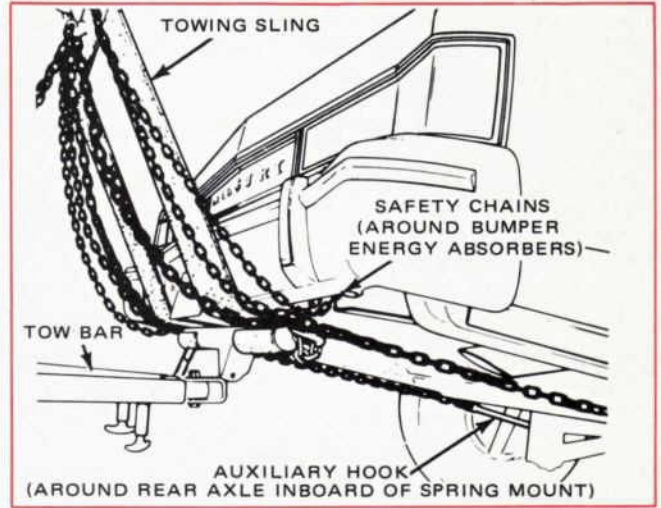


Figure 27 — Mercury — Rear

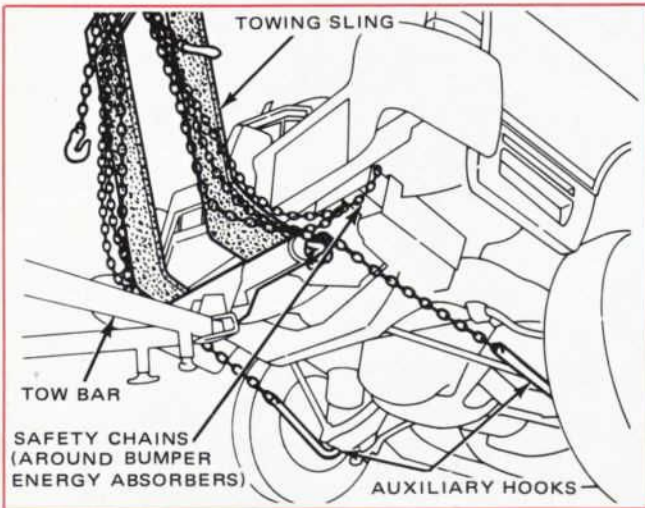


Figure 28 — Lincoln Continental — Front

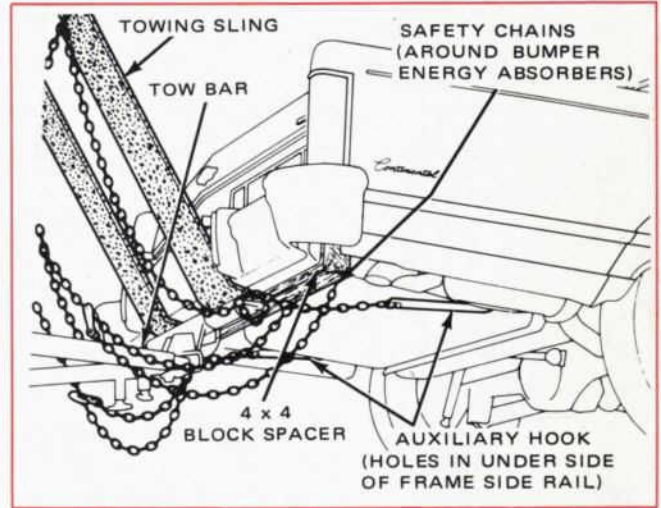


Figure 29 — Lincoln Continental — Rear

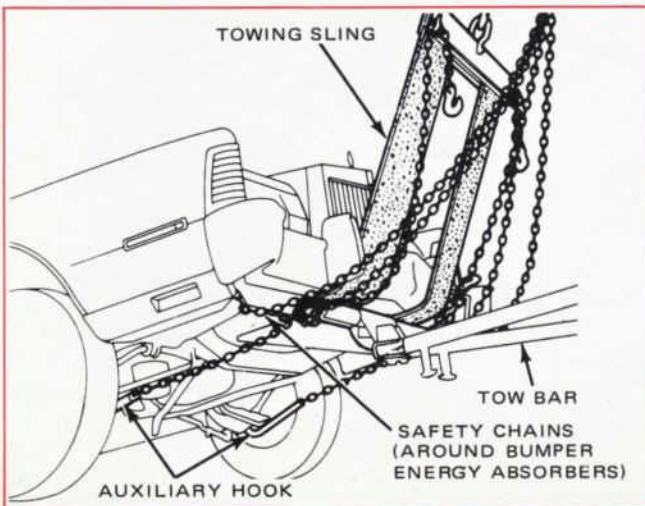


Figure 30 — Continental Mark IV — Front

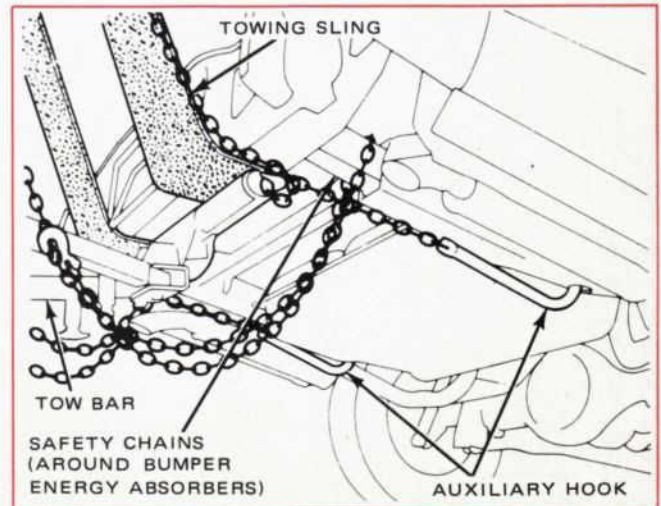


Figure 31 — Continental Mark IV — Rear

EMERGENCY TOWING, HOISTING and STARTING

GENERAL HOISTING INSTRUCTIONS

Damage to suspension, exhaust and/or steering linkage components may occur if care is not exercised when positioning the hoist adapters of 2-post hoists prior to lifting the car. If a 2-post hoist is used to lift the vehicle, place the adapters under the front suspension lower arms. *Do not allow the adapters to contact the steering linkage.*

On Lincoln Continental, Continental Mark IV, Thunderbird, Montego, Torino, Elite and Cougar, the rear suspension lower control arms prevent positioning of fork lifts outboard of the suspension lower arms. Place fork lifts under the axle housing, inboard of the suspension arm brackets.

CAUTION: Do not position hoist pads under the Number 3 crossmember on Mark IV, Thunderbird, Montego, Torino and Elite models.

SPECIAL NOTE: Maverick, Comet, Granada, Monarch, Pinto, Bobcat and Mustang II models have a unitized body-frame construction. These vehicles require special precautions and procedures when they're hoisted or jacked up. In some cases, special hoist adapters must be used as recommended by specific hoist manufacturers.

To unload suspension ball joints, position jacks as shown in Figure 32.

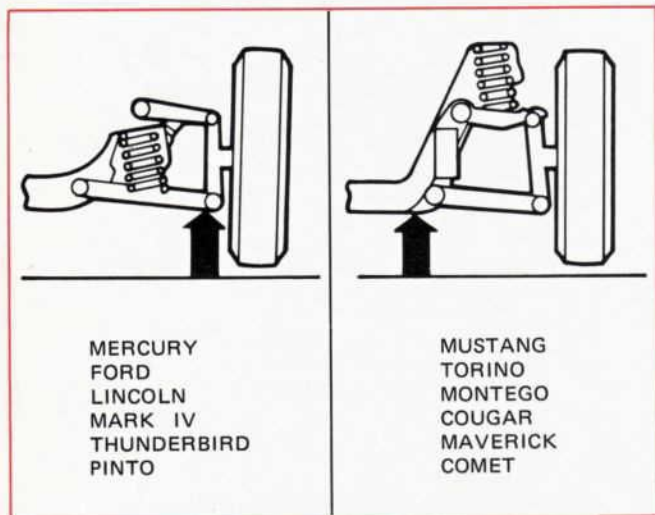


Figure 32 — Front Suspension Lift Points for Ball Joint Unloading.

FLOOR JACK PRECAUTIONS Pinto, Bobcat and Mustang II

These vehicles may be raised at several locations with a floor jack. The front of the car may be raised by positioning the jack under the center of the Number Two crossmember. The front, as well as either side of the rear end, may be lifted by positioning the floor jack under the rocker flange at the contact points used for the jack supplied with the vehicle. To lift both sides of the rear at once, position the floor jack under the differential housing.

CAUTION: Do not use jack pressure on front or rear bumpers.

All Models, Except Pinto, Bobcat and Mustang II

When a stationary floor jack or a roll jack is used, there are several recommended contact points. Either side of the front of the vehicle may be raised by jack contact at the lower control arm strut connection, or by jack pressure on the front crossmember; or on the crossmember to which the stabilizer is connected. On Continental Mark IV, Thunderbird, Montego, Elite and Torino models, position the jack under the rear axle housing between the suspension arm brackets and the differential housing. Do not place jacks under the suspension arm brackets.

CAUTION: Do not use jack pressure on front or rear bumpers of Maverick, Comet, Granada or Monarch.

DRIVE-ON TYPE HOIST PRECAUTIONS

To prevent possible damage to the underbody, NEVER drive the vehicle onto a drive-on type hoist without first checking for possible interference between the upright flanges of the hoist rails and the underbody. If you locate interference, modify the hoist flanges as necessary and/or build up the approach ramps to provide the necessary clearance.

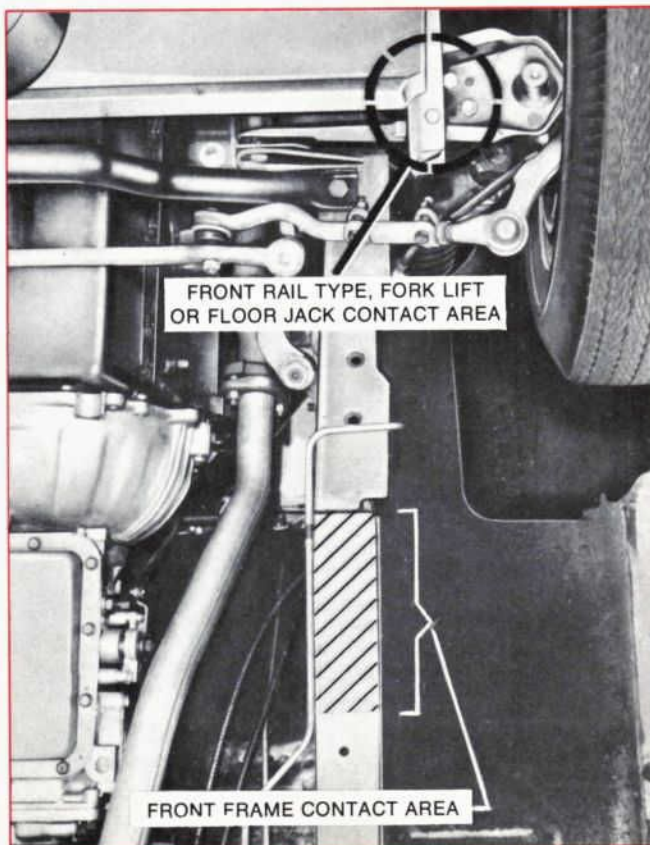


Figure 33 — Front Hoist Contact Areas — Typical

EMERGENCY TOWING, HOISTING and STARTING

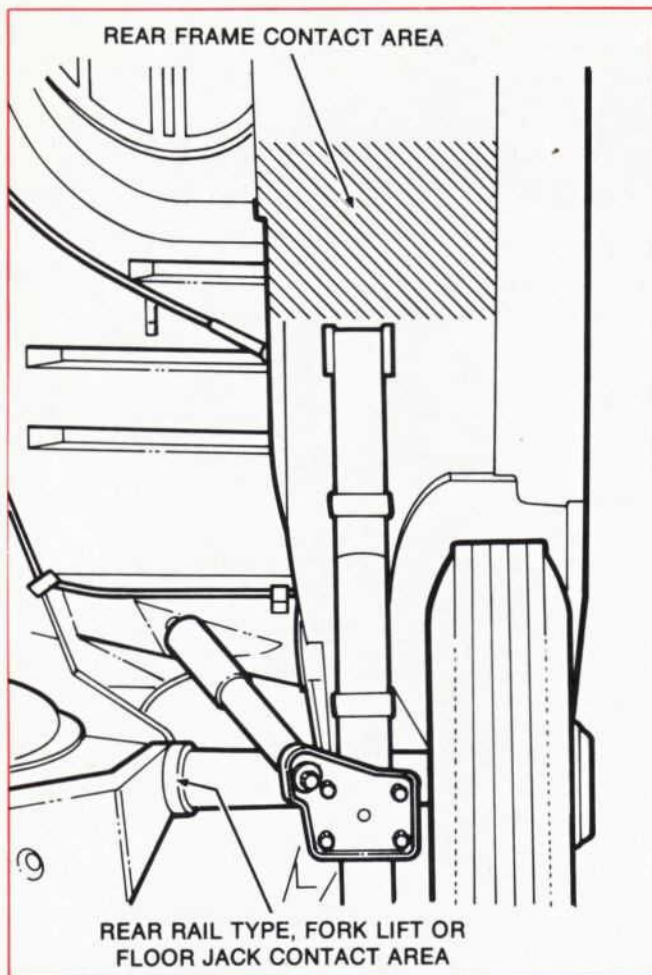


Figure 34 — Rear Hoist Contact Areas — typical

TWIN POST HOIST PRECAUTIONS

To assure safe hoisting, carefully position the front post adapters to contact the center of the lower suspension arms (Figure 33). To prevent damage to the rear shock absorbers, the rear forks must contact the axle at points no more than one-inch outboard from the circumference welds near the differential housing. Carefully raise the rear post and check the position of the fork as shown in Figure 34.

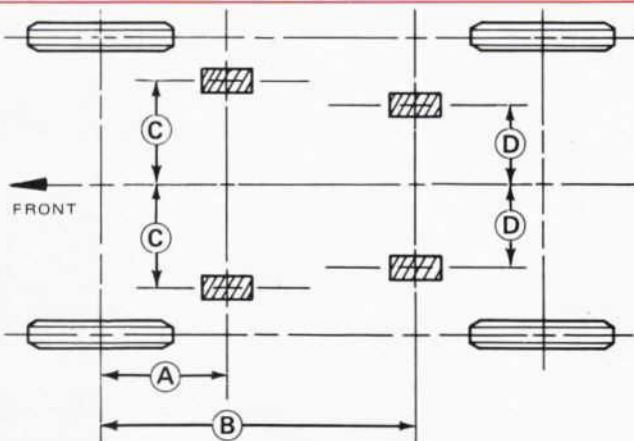
RAIL-TYPE FREE WHEELING HOIST PRECAUTIONS

Carefully position the front adapters or hoist plates in contact with the lower suspension arms to assure safe, accurate lifting. Carefully position the rear hoist adapters under the rear axle to prevent damage to the shock absorbers when the vehicle is raised. Raise the hoist rails slowly and check the position of the adapters.

FRAME CONTACT HOIST PRECAUTIONS

Adapters are necessary to lift the vehicle with frame contact hoists, and must be placed at the four contact points shown in Figure 35. Position the adapters so that they are centered on the adapters contact area. *All four contact points must contact the adapters.*

NOTE: Exercise care when hoisting vehicles equipped with catalytic converters, to assure necessary clearance between hoist and exhaust system components, before energizing the hoist.



Dimension (Inches)	Car Line												
	Comet	Cougar	Continental Mark IV	Ford	Lincoln Continental	Maverick	Mercury	Montego	Mustang	Pinto Bobcat	Thunderbird	Torino Elite	Granada Monarch
(A)	21.3	22.0	24.9	21.5	21.7	21.3	21.5	22.0	17.5	17.5	24.9	22.0	21.5
(B)		94.5	101.0	97.5	103.7		100.5	94.5 ^① 98.5 ^②			101.0	94.5 ^① 98.5 ^②	83.5
(C)	20.0	25.0	25.0	23.0	23.5	20.0	23.0	25.0	12.0	12.0	25.0	25.0	26.4
(D)	22.0	27.5	26.0	25.0	27.0	22.0	25.0	27.5	24.0	24.0	26.0	27.5	26.4

① 2 Door. ② 4 Door.

Figure 35 — Frame Contact Lift Points

EMERGENCY TOWING, HOISTING and STARTING

EMERGENCY STARTING TIPS

CAUTION: Avoid attempting to start a vehicle by pushing. Instead, use jumper cables as described below.

Jump Starting Vehicles with "Dead" Battery .

When starting vehicles with another battery and jumper cables (Fig. 36), damage may occur to the electrical system components such as solid state modules, light bulbs, gauges, etc., if the following instructions are not adhered to:

WARNING: Batteries contain SULFURIC ACID. Shield your eyes when working near the battery to protect against possible spilling of the acid solution. In case of acid contact with skin, eyes or clothing, FLUSH IMMEDIATELY WITH WATER FOR A MINIMUM OF FIVE MINUTES. Get medical attention immediately.

Hydrogen and oxygen gases are produced during normal battery operation. This gas mixture can explode if flames or sparks are brought near the battery. When charging or using a battery in an enclosed space, always provide ventilation, and no smoking.

1. Do not use source voltages higher than 12 volts.

CAUTION: Use only a 12-volt jumper system. You can damage a 12-volt starter motor and the ignition system beyond repair (even when cranking loads are relatively light) by connecting it to a 24-volt power supply (two 12-volt batteries in series, or a 24-volt motor generator set). The likelihood of extensive damage is greatly increased if the starter is connected to a 24-volt power supply while being subjected to prolonged and heavy cranking loads such as attempting to start an engine in subzero weather.

2. Turn all battery loads (e.g., lights, heater, A/C blower) "off" during starting.
3. Remove vent caps and put protective cloth cover over battery.

NOTES:

- Some batteries are the maintenance-free type and do not have vent caps.
 - Use particular care when connecting a booster battery to prevent sparks.
4. Connect ends of one cable to positive (+) terminals of each battery.
 5. Connect one end of the other cable to negative (-) terminal of "good" battery.
 6. Connect other end of cable to engine block on vehicle being started (NOT TO NEGATIVE (-) TERMINAL OF BATTERY).

NOTE: Follow the starting procedure indicated in the Owners Manual. To help avoid possible injury or damage to the vehicle, observe the precautions listed in the Emission System part of the "Getting To Know Your Car" section of the Owners Manual.

7. After the car has started and before disconnecting jumper cables, the following must be adhered to:
 - Make certain that engine reaches idle speed.
 - Turn "on" the following battery loads:
 - Heater — A/C blower
 - Headlights hi-beam
 - Heated backlite (if so equipped)

THIS STEP IS REQUIRED TO PREVENT ANY VOLTAGE SURGE, WHICH COULD DAMAGE OTHER ELECTRICAL COMPONENTS, WHILE DISCONNECTING JUMPER CABLES.

8. Turn all battery loads "off" after disconnecting jumper cables.
9. Replace the vent caps and carefully dispose of the cloth that covered the battery since it may contain sulfuric acid.

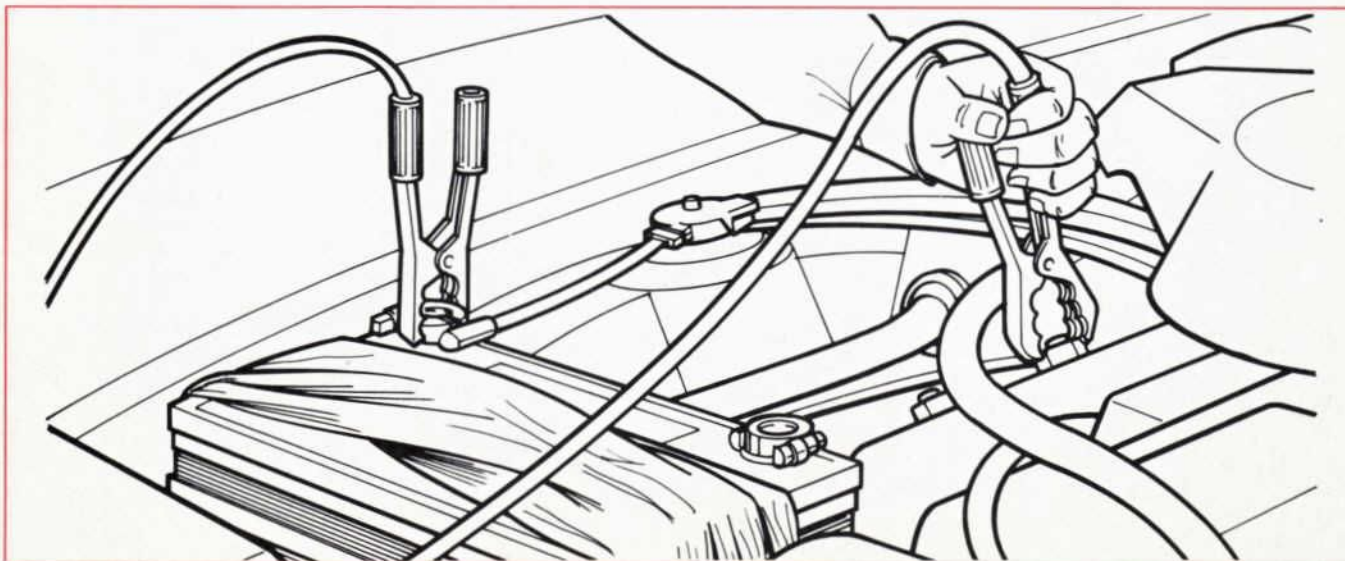


Figure 36 — Jump Starting Dead Battery

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