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SHOP TIPS

Autolite



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FORD SERVICE PUBLICATIONS!

Available from Ford Motor Company
Complete listing in this Issue



Technical parts and service information published by the Autolite-Ford Parts Division and distributed by Ford and Lincoln-Mercury dealers to assist servicemen in Service Stations, Independent Garages and Fleets.

- PRE-VACATION MAINTENANCE TIPS AND ACCESSORIES FOR TRAILER TOWING
- COMPLETE LISTING AND DESCRIPTION OF SERVICE PUBLICATIONS AVAILABLE FROM FORD
- INSTALLING THE NEOPRENE RUBBER VALVE COVER GASKET

VACATION TIME IS



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Be sure and file this and future bulletins for ready reference. If you have any suggestions for additional information that you would like to see included in this publication, please write to: Autolite-Ford Parts Division of Ford Motor Company, Merchandising Services Dept., P.O. Box 3000, Livonia, Michigan 48151.

The descriptions and specifications contained in this book were in effect at the time the publication was approved for printing. The Ford Motor Company, whose policy is one of continuous improvement, reserves the right to discontinue models at any time, or to change specifications or design without notice and without incurring obligation.

Autolite 

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DEARBORN, MICHIGAN

VOL. 69 MSD 45 LITHO IN U.S.A.

FOR MODERN AMERICANS, THE "GOING THING" IS TO HEAD FOR VACATION ADVENTURE WITH TRAVEL TRAILER, BOAT OR CAMPER IN TOW!



This summer you'll see them throughout this great land of ours—modern American families on the move with gleaming-new travel trailers, boats and campers in tow behind their luggage-laden cars.

A Lincoln-Continental skimming across golden prairies, pulling a sun-speckled silver trailer with all the rich comforts of a city apartment.

A Falcon in Idaho's incredibly beautiful Farragut area, a rugged tent-camper on its hitch.

A Mercury Station Wagon with a crew of fishermen, and their broad-beamed boat, following the Coho salmon's course northward up Lake Michigan's shore.

A Ford parked with a trailer at Fontana in the Smokies.

A Mustang towing a mini-boat along a beachside California highway. Some of these vacationers are following trails they've traveled many times before, to places and activities long familiar to them and to their parents before them.

Many are living the outdoors for the first time.

Whether old-timer or novice, they've learned, or must soon learn, that their brand of fun often makes extra demands of their cars. Camping families seldom travel light. Hunters and fishermen seldom travel alone. Scenic attractions are seldom close at hand. Out-of-the-way places seldom offer the smooth going of city boulevards—or a mechanic around the next corner.

So they'll need automobiles properly equipped and properly serviced to make their campground dreams become pleasant memories.

Which is where you come in.

TRAILER-TOWING TIME!



THIS OUTDOOR RECREATION BOOM MEANS MORE BUSINESS FOR YOU!

**Vacationing trailerists, campers and boaters will look to you for
needed services, products and sound advice!**

Basic starting point for the right car to do the right job for the outdoors enthusiast obviously is the enthusiast's favorite automobile dealer.

At Ford and Lincoln-Mercury Dealerships — traditional favorites for the outdoors set—the car buyer will especially find the information and the personnel to provide him with the right recreation vehicle.

However, whether at home or on the road, many of these motorists will be coming to you for service, Autolite parts and accessories, and other automotive products.

And, often times, *for your advice.*

For you, this represents, not a *burden*, but an *opportunity* for better customer relations and continued *profitable* service work.

By steering that customer right about his car . . . by seeing

to it that his tires, cooling system, battery and engine are in good shape for a summer of towing . . . or by making sure he's using an adequate trailer hitch . . .

. . . You'll be providing the kind of *total service* that makes friends among the kind of people who are willing to pay for the extra service they seek.

To help you do this job, you'll find on the following six pages key auto service checks for vacationing recreation enthusiasts, plus towing recommendations for all Ford and Lincoln-Mercury passenger cars.

Your nearby Ford or Lincoln-Mercury Dealer can assist with individual answers to any questions you or your customers may have regarding towing.

Put the information to good use and you'll put new profits into action!



VACATION TIME IS

HAPPY VACATION TRAILERING CALLS FOR . . .

. . . THE RIGHT EQUIPMENT . . .

Ford Motor Company cars can tow trailers of Class I size (maximum trailer weight up to 2,000 pounds, static tongue load 200 pounds) with very little special equipment.

In most cases, all that is needed is a Ford Hitch and a Ford Wiring Kit (which includes a four-wire connector plug and a heavy-duty turn signal flasher).

Recommendations for larger trailers include: increased wheel and tire sizes, heavy-duty suspension, air lift springs and a load-equalizing trailer hitch. Automatic transmissions are preferred over manual transmissions for towing.

Towing recommendation charts on Pages 6-9 provide additional details. For further information and special circumstances, check with your nearby Ford or Lincoln-Mercury Dealer.

Tips On Trailer Hitches

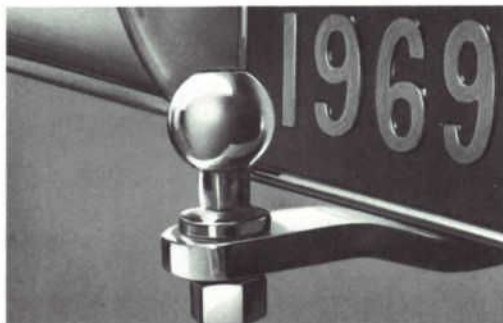


Figure 1—A Typical Ford Hitch Installation

Although many varieties of coupling devices are available, each with special applications, Ford recommends the use of frame hitches only.

For trailers with static tongue loads under 200 pounds, the connection can be a simple hitch attached to the frame. With a heavier load, the hitch must spread the load between the trailer and towing vehicle. Then it is necessary to install a load-equalizing hitch that attaches to the frame and distributes part of the load back to the trailer wheels. This type of hitch equalizes the tongue load (weight resting on the car hitch) to the trailer and car wheels. Thus, it helps keep the towing vehicle and trailer level for easier handling, safer and more comfortable driving.

Most equalizing frame hitches can be adjusted for height when installed, so that the trailer will ride level with the towing vehicle.

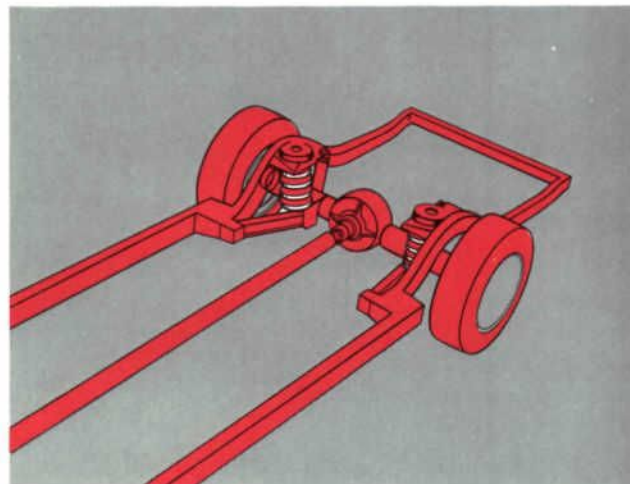
NOTE: Axle hitches are not recommended in *any* case.

Safety chains always should be crossed under the tongue to prevent the tongue from dropping to the ground in the event of a coupling failure. The following coil-steel welded chains are recommended:

TRAILER CLASS	TRADE SIZE
I	$\frac{3}{16}$ inch
II	$\frac{1}{4}$ inch
III	$\frac{5}{16}$ inch

Mighty useful accessories for those towing larger trailers are Air Lift Spring Kits or Autolite "Super-Flex" shock absorbers.

"HELPER" SPRINGS



TYPE II AIR LIFT SPRING KIT



AUTOLITE "SUPER-FLEX" SHOCK ABSORBER

Figure 2—"Helper" Springs

How They Work. Ford adjustable Air Springs are tough butyl rubber tanks that fit inside rear coil springs and over the top of leaf springs. When inflated, they bond to the springs and stiffen their action. They are adjustable. The more air in the tank, the more the spring's action is bolstered. A car's suspension can be tailored to suit exactly the load it carries. When the overload is gone it's easy to deflate the tanks and return the suspension to normal. Air pressure can be provided by a hand pump or at a service station.

SUPER-FLEX—3-STAGE LOAD BOOSTERS . . . for severe load requirements. The industry's only automatic three-stage Heavy Duty shock absorber. Gives you normal shock absorber ride—no "Tail up" look during normal use. Provides full over-load protection for almost a half-ton of extra weight.

TRAILER-TOWING TIME!

... AND PROPER SERVICE

Ford-built cars require very little special attention. They are built to operate under a wide range of conditions and boast trend-setting extended-interval maintenance requirements.

However, extreme usage situations arise when the car owner loads his car with family and vacation gear, attaches a trailer, and travels for long periods at freeway speeds or on secondary roads and trails. Under these conditions, in addition to some useful added accessories, a few special services are advisable to provide a safe, trouble-free trip.

Here's where you can provide your most valuable service to the vacation-bound motorist and gain new opportunities for added service sales.

Tires—Special Attention Needed

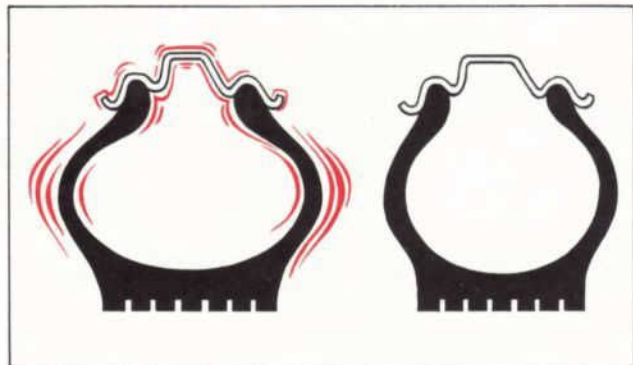


Figure 3—Tire Pressure Vs. Load-Carrying Capacity

Tires probably require more special attention by vacationing trailerists than any other item, because of the increased loads carried.

Load carrying capacities can be increased by using larger tires or by increasing tire pressure (up to recommended limits). Using underinflated tires can result in steering and stability problems due to weak sidewalls (Figure 3).

The tires specified as standard equipment on all Ford-built cars can be used under widely ranging conditions by varying tire pressure. The exception is when extremely heavy loads are carried, in which case oversize tires may be required.

Cooling System—Another Vital Check

All Ford Motor Company vehicles are filled with Ford Permanent Anti-Freeze at the factory. If any other type has been used since the original fill and has been in the car more than one year, recommend a complete drain and flush. Be sure to add a rust inhibitor.

The coolant level should be maintained at about one inch below the bottom of the inlet filler neck ring (Figure 4).

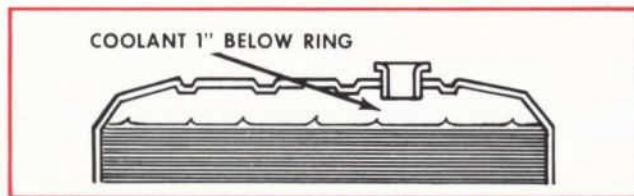


Figure 4—Coolant Level

The cooling system should maintain a pressure of 12-15 psi. If the system will not hold 12-15 pounds, or if coolant must be added more than once a month, or more than a quart at one time, check the cap, radiator filler neck, hose connections, and other areas for leakage.

To avoid injury when checking a hot engine, do not immediately remove the radiator cap. Muffle the cap in a thick cloth and turn it counter-clockwise only until the pressure starts to escape. After the pressure has completely dissipated, finish removing the cap. If the coolant is low, add coolant gradually, with the engine running.

Engine—For Peak Performance

For the vacationer to obtain peak performance and top efficiency from his car during the many miles of summer travel, his engine should be in top running order.

If the car will soon be due for a scheduled tune-up, recommend one. And recommend that the motorist replace with top-quality Autolite original equipment parts—to include: spark plugs, wire and cable, distributor parts, PCV valve and air and fuel filters.

If the car is near the oil and filter change interval, recommend high-quality replacements, such as the Autolite 6,000-Mile Oil Filter and a motor oil that meets Ford specifications. This will continue the high level of protection provided every Ford-built car at the factory.

Electrical System—To Avoid Failure

Extensive use of power-draining accessories during outdoor vacations makes a check of the electrical system another vital part of your pre-vacation services.

The most important component in the electrical system is the battery. Check for cracks or buckling that can lead to failure. The top should be clean and dry. If wet, dirty, or acid-soaked, the battery will constantly discharge. Also, check the cables for looseness, corrosion and wear. Last, but most important, check battery capacity.

Capacity is a measure of the battery's ability to furnish current and maintain minimum necessary voltage. If the battery passes the capacity test, it is in satisfactory condition. However, if the specific gravity is below 1.230, it should be recharged to bring the battery to peak performance.

Fan Belts, Too

Check each fan belt for signs of fraying or breaks. If any belt appears to be of doubtful condition, replace it with the proper size belt. This is important, because if one belt flips or breaks it may damage other belts. This could result in an inoperative car. Be sure each belt is adjusted to the proper tension.



VACATION TIME IS

1969 FORD TOWING RECOMMENDATIONS

SAE TRAILER CLASS	CLASS I*	CLASS II	CLASS III
Fully Loaded Trailer Weight**	Up to 2000 lb.	2000 to 3500 lb.	3500 to 5000 lb.
Static Tongue Load	Up to 200 lb.	200 to 500 lb.	500 to 600 lb.
Engine—cu.-in.	240 Six (M), 302 V-8 (R)	302 V-8 (M), 390-2V V-8 (R)	390-2V V-8 (M), 429-2V or 4V V-8 (R)
Transmission	Cruise-O-Matic	Cruise-O-Matic	Cruise-O-Matic (b)
Axle Ratio	2.80:1	3.25:1	3.25:1
Engine Cooling	Standard (M), Extra Cooling Package (R)	Standard (M), Extra Cooling Package (R)	Extra Cooling Package (c)
Tires, Wheels (f)			
Station Wagon	8.55 x 15 (std.)	8.55 x 15 (std.)	8.55 x 15 (std.)
All Others	7.75 x 15 (std.)	7.75 x 15 (M), 8.55 x 15 (R)	8.55 x 15 (b)
Suspension (springs and shocks)	Standard	Heavy-Duty	Heavy Duty (c)
Brakes	Standard (M), Power Front Disc (R)	Power Front Disc	Power Front Disc (c)
Steering	Standard (M), Power Assist (R)	Power Assist	Power Assist
Ride Control (d)	Automatic (R)	—	—
Alternator	Standard	Standard (M), 55 Amp. (R)	55 Amp. (M), 65 Amp. (R)
Battery	Standard	Standard (M), 55 Amp.-Hr. (R)	70 Amp.-Hr. (M), 80 Amp.-Hr. (R)
Hitch	Non-Equalizing (d)	Load-Equalizing Frame-Attached (e)	Load-Equalizing Frame-Attached (e)

NOTES:

- (M) Minimum.
- (R) Recommended.
- (b) Required.
- (c) Included in Trailing Special Package.
- (d) Accessory item, not recommended with load-equalizing hitch.

- (e) Load-equalizing frame attached trailer hitch is required for all Class II and III trailers with tongue load over 200 lbs. Axle attached hitches are not recommended.
- (f) Standard tires vary with series, model and equipment.

*Trailers with gross weight of less than 1,000 lbs. can be towed with no special equipment other than a proper trailer hitch.

**Trailer brakes are required for all trailers over 1,500 lbs. in all states, and for over 1,200 lbs. in some states. Do not connect hydraulic brake lines from car brake system to trailer system.

1969 FAIRLANE TOWING RECOMMENDATIONS

SAE TRAILER CLASS		CLASS I	CLASS II	
Fully Loaded Trailer Weight		Up to 2000 lb.	Up to 3000 lb.	Up to 3500 lb.
Static Tongue Load		Up to 200 lb.	Up to 350 lb.	Up to 350 lb.
Optional Equipment	Minimum	Recommended	Minimum	Recommended
Engine—cu.-in.	250 Six	302 V-8	351-2V V-8	351-2V V-8
Transmission	Cruise-O-Matic	Cruise-O-Matic	Cruise-O-Matic	Cruise-O-Matic
Axle Ratio (a)	3.25:1	3.00:1	3.00:1	3.25:1 (c)
Engine Cooling	Standard	Standard	Extra Cooling Package (b)	Extra Cooling Package (b)
Tires and Wheels (f)	7.75 x 14 (std.)	7.75 x 14 (std.)	7.75 x 14 (std.)	7.75 x 14 (std.)
Suspension (springs and shocks)	Standard	Standard	Heavy-Duty (c)	Heavy-Duty (c)
Ride Control	—	Manual (d)	—	—
Steering	Standard	Power Assist	Power Assist	Power Assist
Brakes	Standard	Power Front Disc	Power Front Disc (c)	Power Front Disc (c)
Alternator	Standard	Standard	55 Amp. (c)	55 Amp. (c)
Battery	Standard	Standard	55 Amp.-Hr. (c)	55 Amp.-Hr. (c)
Hitch	Non-Equalizing (d)	Non-Equalizing (d)	Load-Equalizing Frame Attached (e)	Load-Equalizing Frame Attached (e)

NOTES:

- (a) Limited-Slip differential available.
- (b) Includes radiator, fan and pulley combinations necessary to meet cooling requirements, available with air-conditioning only. Included in Trailing Special Package.

- (c) Included in Trailing Special Package. (3.00:1 axle ratio with air-conditioning).
- (d) Accessory item.
- (e) Load-equalizing hitch recommended for trailers close to 2000 lbs. and 200-lb. tongue load.
- (f) Standard tires vary with series, model and equipment.

TRAILER-TOWING TIME!

1969 THUNDERBIRD TOWING RECOMMENDATIONS

SAE TRAILER CLASS	CLASS I	CLASS II	CLASS III
Fully Loaded Trailer Weight	Up to 2000 lb.	2000 to 3500 lb.	3500 to 5000 lb.
Static Tongue Load	Up to 200 lb.	200 to 500 lb.	Up to 600 lb.
Axle Ratio (a)	2.80:1	3.00:1	3.00:1
Engine Cooling	Standard	Extra Cooling Package (b)	Extra Cooling Package (b)
Tires, Wheels	8.15 x 15 (std.)	8.15 x 15 (std.)	8.15 x 15 (std.)
Suspension (springs and shocks)	Standard	Heavy-Duty	Heavy-Duty
Ride Control (c)	Automatic	—	—
Hitch	Non-Equalizing (d)	Load-Equalizing Frame Attached (e)	Load-Equalizing Frame Attached (e)

*Standard 1969 Thunderbird equipment includes 429-4V V-8 Engine, Cruise-O-Matic Transmission, Power Front Disc Brakes and Power Steering adequate for trailer classes shown.

NOTES:

- (a) Limited-Slip differential available.
- (b) Available with Air-Conditioning only.
- (c) Not recommended with load-equalizing trailer hitch.
- (d) Accessory item, not recommended with load-equalizing hitch.
- (e) Load-equalizing frame-attached trailer hitch is required for all Class II and III trailers with tongue load over 200 lbs. Axle-attached hitches are not recommended.

1969 MUSTANG TOWING RECOMMENDATIONS

SAE TRAILER CLASS		CLASS I
Fully Loaded Trailer Weight		Up to 2000 lbs.
Static Tongue Load		Up to 200 lbs.
Optional Equipment	Minimum	Recommended
Engine—cu.-in.	250 Six	351-2V V-8
Transmission	Cruise-O-Matic	Cruise-O-Matic
Axle Ratio (b)	2.79:1	3.25:1
Engine Cooling	Standard	Extra Cooling Package
Tires, Wheels (c)	C78 x 14 (std.)	E70 x 14 (std.)
Brakes	Standard	Power Front Disc
Steering	Standard	Power Assist
Ride Control	—	Manual (a)
Hitch	Non-Equalizing (a)	Non-Equalizing (a)

NOTES:

- (a) Accessory item. (b) Limited-Slip differential available. (c) Standard tires vary with model and equipment.

1969 FALCON TOWING RECOMMENDATIONS

SAE TRAILER CLASS		CLASS I
Fully Loaded Trailer Weight	Up to 1000 lb.	1000 to 2000 lb.
Static Tongue Load	Up to 100 lb.	100 to 200 lb.
Optional Equipment	Minimum	Recommended
Engine—cu.-in.	200 Six	302 V-8
Transmission	Cruise-O-Matic	Cruise-O-Matic
Axle Ratio (c)	3.08:1	3.00:1
Engine Cooling	Standard	Extra Cooling Package (a)
Tires, Station Wagon	7.75 x 14 (std.)	7.75 x 14 (std.)
Wheels (d) All Others	6.95 x 14 (std.)	7.35 x 14 (opt.)
Brakes	Standard	Power Front Disc
Steering	Standard	Power Assist
Ride Control	—	Manual (b)
Alternator	Standard	42 Amp.
Battery	Standard	55 Amp.-Hr.
Hitch	Non-Equalizing (b)	Non-Equalizing (b)

NOTES:

- (a) Available with Air-Conditioning only.
- (b) Accessory item.

- (c) Limited-Slip differential available.
- (d) Standard tires vary with model and equipment.



VACATION TIME IS

1969 MERCURY TOWING RECOMMENDATIONS

SAE TRAILER CLASS	CLASS I—LIGHT DUTY*		CLASS II—MEDIUM DUTY		CLASS III—HEAVY DUTY	
	Minimum Equipment	Recommended Equipment	Minimum Equipment	Recommended Equipment	Minimum Equipment	Recommended Equipment
Gross Trailer Weight** (loaded)	Up to 2000 lbs.		Up to 3500 lbs.		Up to 5000 lbs.	
Static Tongue Load	Up to 200 lbs.		Up to 500 lbs.		Up to 600 lbs.	
Engine	Any Engine		Any Engine	429-2V or -4V	Any Engine	429-2V or -4V
Axle Ratio	Any Ratio		3.25:1	3.25:1	3.25:1	3.25:1(a)
Transmission	Select-Shift		Select-Shift		Select-Shift	
Engine Cooling	Standard	Extra Cooling	Extra Cooling		Extra Cooling	
Wheels—Passenger —Station Wagon	Standard Standard		Standard Standard		Standard Standard	
Tires—Passenger —Station Wagon	Standard	8.55 x 15 Standard	Standard	8.55 x 15 Standard	8.55 x 15 Standard	
Brakes (b)	Standard	Power Disc	Power Disc		Power Disc	
Suspension—Springs and Shocks	Standard		Heavy-Duty		Heavy-Duty	
Steering	Standard	Power Assist	Power Assist		Power Assist	
Ride Control (c)	(c)		—		—	
Alternator	Standard		Standard	55 amp.	55 amp.	65 amp.
Battery	Standard		Standard	70 amp.-hr.	70 amp.-hr.	80 amp.-hr.
Trailer Hitch—Type	Non-Equalizing (d)		Weight-Distributing		Weight-Distributing	

NOTES:

- (a) 3.00:1 axle mandatory with 429 V-8 (4V) engine.
 (b) Power brakes (front disc and rear drum) are recommended for towing gross loads of more than 1,000 lbs.
 (c) Adjustable Air Springs are recommended for Class I towing with a non-equalizing hitch. These are factory-installed as a limited production option for \$42.80 extra cost, or are available as a dealer-installed accessory. The Automatic Ride Control option is not recommended with the weight-distributing hitch.

(d) The Lincoln-Mercury Trailer Hitch designed for Mercury Monterey-Marquis-Marauder vehicles is available as a dealer accessory. Optional items are extra-cost equipment.

*Trailers with gross weight of less than 1,000 lbs. can be towed with no special equipment other than a proper trailer hitch.

**Trailer brakes are required for all trailers over 1,500 lbs. in all states, and for over 1,200 lbs. in some states. Do not connect hydraulic brake lines from car brake system to trailer system.

1969 MONTEGO TOWING RECOMMENDATIONS

SAE TRAILER CLASS	CLASS I—LIGHT DUTY*		CLASS II—MEDIUM DUTY	
	Minimum Equipment	Recommended Equipment	Minimum Equipment	Recommended Equipment
Gross Trailer Weight (loaded)**	Up to 2000 lbs.		Up to 3500 lbs.	
Static Tongue Load	Up to 200 lbs.		Up to 350 lbs.	
Engine	Any Engine		Any V-8 351-2V or Above	
Axle Ratio	3.25:1		3.00:1	3.25:1
Transmission	Select-Shift		Select-Shift	
Engine Cooling	Standard		Extra Cooling	
Tires—Passenger Cars —Station Wagon	Standard Standard		Standard Standard	
Brakes	Standard	Power Disc	Power Disc	
Suspension—Springs and Shocks	Standard		Cross-Country Ride Package (a)	
Steering	Standard	Power Assist	Power Assist	
Adjustable Air Springs	—		(b)	
Alternator	Standard		55 Amp.	
Battery	Standard		Heavy-Duty	
Trailer Hitch—Type	Non-Equalizing (c)		Load-Equalizing	

NOTES: (a) Cross-country Ride Package includes higher rate front and rear springs and shock absorbers. Not available with CJ 428 V-8. (b) Adjustable air springs are recommended for Class I towing with the non-equalizing hitch. These are available as a dealer-installed accessory. (c) The Lincoln-Mercury Trailer Hitch designed for Montego Class vehicles is available as a dealer accessory.

Optional items are extra-cost equipment.

*Trailers with gross weight of less than 1,000 lbs. can be towed with no special equipment other than a proper trailer hitch.

**Trailer brakes are required for all trailers over 1,500 lbs. in all states, and for over 1,200 lbs. in some states. Do not connect hydraulic brake lines from car brake system to trailer system.

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and it can all be yours FREE!*

A unique opportunity for you to build your profits through service with fast-selling Autolite Starter Pack Parts, and qualify for FREE Autolite cabinet merchandisers, sales aids, and the all-new year-round Autolite Service Information Plan!

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Autolite products you want! The only requirement is that the order includes a minimum of three Autolite product lines and meets the required dollar value at suggested Stocking Dealer prices.

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Here's a parts cabinet that makes sense! Puts everything together for you, and you can get it free!



The strongest, most attractive merchandiser available anywhere! Giant capacity and dispenser doors are only two of the many special features that are yours with this cabinet. Custom design accommodates all your fast-moving Autolite parts and lets you locate them quickly and easily. You qualify by ordering a Starter Pack with a minimum of three Autolite product lines that total \$400.00 or more at suggested Stocking Dealer Prices.

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that fits your requirements...and you're in!

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CONTACT—Autolite Service Centers receive this lively magazine six times a year. The latest information on Autolite Merchandising Programs, sales aids, and new products are highlighted. Feature stories cover a broad variety of topics including sports, high performance, and the latest business trends.

AUTOLITE ALL-PRODUCTS CATALOG—All make part numbers and application information for all Autolite product lines . . . with a "Fast-Movers" Application Section for speedy reference; the easy way to quickly select the right part for any car. Autolite Service Centers get the current catalog automatically each year.

AUTOLITE-FORD READY REFERENCE CATALOG—Issued annually to Autolite Service Centers, this catalog provides a comprehensive list of all fast-moving Ford and Autolite parts for Ford Motor Company vehicles.

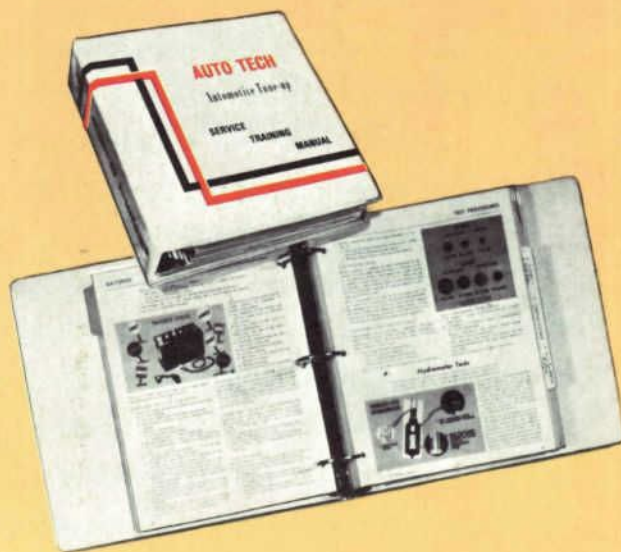
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TRAILER-TOWING TIME!

1969 LINCOLN CONTINENTAL TOWING RECOMMENDATIONS

SAE TRAILER CLASS	CLASS I*—LIGHT	CLASS II*—MEDIUM	CLASS III*—HEAVY
Gross Trailer Weight** (loaded)	Up to 2000 lbs.	Up to 3500 lbs.	Up to 5000 lbs.
Static Tongue Load	Up to 200 lbs.	Up to 500 lbs.	Up to 600 lbs.
Engine	460-4V	460-4V	460-4V
Axle Ratio	Standard	3.00:1(a)	3.00:1(a)
Transmission	Automatic	Automatic	Automatic
Engine Cooling	Standard	Standard	Standard
Wheels & Tires	Standard	Standard	Standard
Brakes	Standard	Standard	Standard
Suspension—Springs and Shocks	Standard	Standard	Heavy-Duty(b)
Steering	Standard	Standard	Standard
Automatic Ride Control (c)	Automatic	—	—
Alternator	Standard	Standard	Standard
Battery	Standard	Standard	Standard
Trailer Hitch-Type	Non-Equalizing(d)	Weight Distributing	Weight Distributing

NOTE: Only the approved inside-the-trunk weight-distributing hitch is recommended for Class II and Class III towing. Only the bolt-on conventional Ford Trailer Hitch or equivalent utility bolt-on type hitches are recommended for Class I towing.

*Trailers with gross weight up to 3500 lbs. and tongue loads up to 500 lbs. can be towed by 1969 Lincoln Continentals with no special equipment other than a proper trailer hitch. **TRAILER

BRAKES are required in most states for trailers weighing more than 1500 lbs., and in some states for trailers weighing more than 1200 lbs. The direct connection of hydraulic brake lines from car brake system to trailer system is strongly discouraged. (a) The optional high torque rear axle with 3.00:1 ratio is no extra cost when factory-installed. (b) Heavy-duty front and rear springs and shock absorbers are optional at extra cost. (c) Automatic Ride Control is not recommended with the weight-distributing hitch. (d) Accessory item.

1969 CONTINENTAL MARK III TOWING RECOMMENDATIONS

SAE TRAILER CLASS	CLASS I*—LIGHT	CLASS II*—MEDIUM	CLASS III*—HEAVY
Gross Weight** (loaded)	Up to 2000 lbs.	Up to 3500 lbs.	Up to 5000 lbs.
Static Tongue Load	Up to 200 lbs.	Up to 500 lbs.	Up to 600 lbs.
Engines	460-4V	460-4V	460-4V
Axle Ratios	Standard	3.00:1(b)	3.00:1(b)
Transmission	Automatic	Automatic	Automatic
Engine Cooling	Standard	Standard	Standard
Wheels & Tires	Standard	Standard	Standard
Brakes	Standard	Standard	Standard
Suspension—Springs and Shocks	Standard	Standard	Cross-Country Ride Package (c) (b)
Steering	Standard	Standard	Standard
Adjustable Air Springs	(a)	—	—
Alternator	Standard	Standard	Standard
Battery	Standard	Standard	Standard
Trailer Hitch-Type	Non-Equalizing(d)	Weight Distributing	Weight Distributing

NOTES: (a) Adjustable Air Springs, available from Lincoln-Mercury dealers as an accessory, are helpful for towing with static tongue loads up to 200 lbs. with the non-equalizing hitch. (Adjustable Air Springs are not recommended for use with a weight-distributing hitch.) (b) Optional at no extra cost, factory installed. (c) Includes standard front and rear springs and heavy-duty front and rear shock absorbers. (d) Dealer-installed accessory.

*Only the approved weight-distributing hitch is recommended for Class II or Class III towing. Bolt-on conventional Lincoln-Mercury Trailer Hitch or equivalent utility bolt-on type hitches

are recommended for Class I towing. Trailers with gross weight from 3,500 to 5,000 lbs. and tongue loads from 500 to 600 lbs. (Class III) can be towed by a 1969 Continental Mark III with no special equipment other than the 3.00:1 rear axle and the Cross-Country Ride Package, both optional at no extra cost, factory-installed, and a proper weight-distributing trailer hitch.

**Trailer brakes are required for all trailers over 1,500 lbs. in all states and for over 1,200 lbs. in some states. Do not connect hydraulic brake lines from car brake system to trailer brake system.

1969 COUGAR TOWING RECOMMENDATIONS

SAE TRAILER CLASS	CLASS I—LIGHT DUTY	
Gross Trailer Weight (loaded)	Up to 2000 lbs.	
Static Tongue Load	Up to 200 lbs.	
	Minimum Equipment	Rec. Equipment
Engine	Any Engine	351-4V or above
Axle Ratio	Any Axle	3.00:1 (d)
Transmission	Select-Shift	
Engine Cooling	Standard	(a)

NOTES: (a) Extra cooling package (LPO). (b) Adjustable Air Springs available as a dealer accessory. (c) Lincoln-Mercury Trailer Hitch, custom-designed for Cougar, available as a dealer accessory. (d) 3.50:1 standard with CJ 428 V-8. Optional items are extra-cost equipment.

SAE TRAILER CLASS	CLASS I—LIGHT DUTY	
	Minimum Equipment	Rec. Equipment
Wheels & Tires	Standard	Standard
Brakes	Standard	Power Disc
Suspension—Springs and Shocks	Standard	Standard
Steering	Standard	Power Assist
Adjustable Air Springs	—	(b)
Alternator	Standard	
Battery	Standard	
Trailer Hitch—Type	Non-Equalizing (c)	

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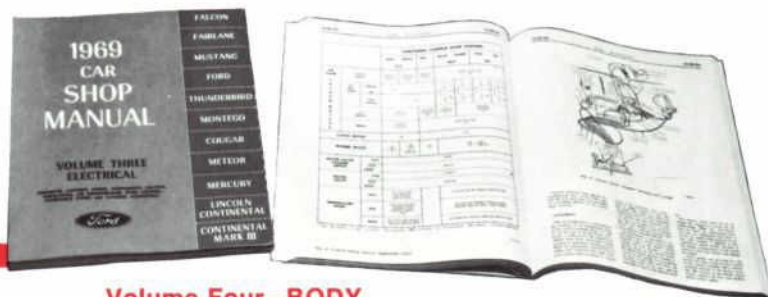
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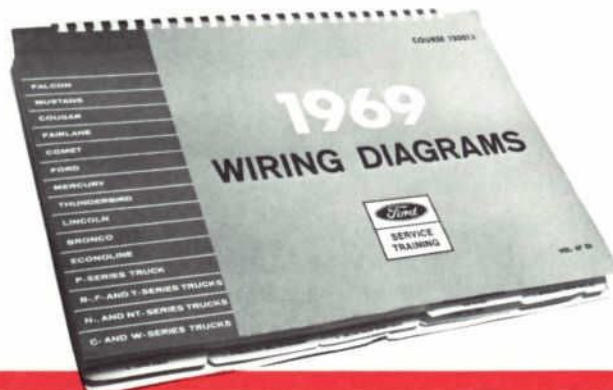
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Bronco/Econoline*	2.25	1.00*
Truck*	3.25	1.50*

*Bought with 1969 Truck Shop Manual (4 volumes).



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1969 Ford Truck Maintenance From Bronco to W-Series Is Simplified With This Four-Volume Shop Manual Set! Only \$9.25

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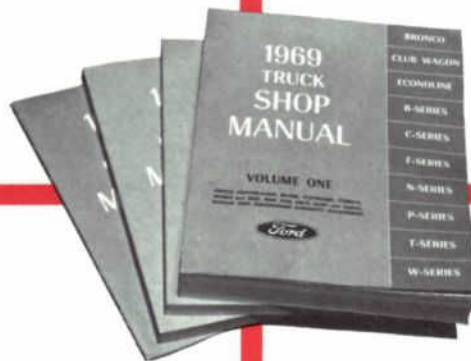
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1969 Ford Truck Service Specifications

Contains all service specifications on 1969 Bronco, Econoline and Ford trucks. For quick reference purposes, each Part has identifying group numbers that correspond with the similar group of the Shop Manuals.



ADDITIONAL FORD CAR AND TRUCK PUBLICATIONS

1948-1968 FORD IMPORT CAR SERVICE PUBLICATIONS

YEAR	MODEL	OWNER'S MANUAL FORM NO.	SHOP MANUAL FORM NO.	PRICE EA.
1948-1953	Anglia, Prefect Sedans Thames ¼ ton Van	AE AEV	Not Available	\$1.25 1.25
1953-1959	Anglia, Prefect Sedans, Escort, Squire Station Wagons Thames ¼ ton Van Shop Manual (bound)	A1 A1V	A1M	1.25 1.25 7.75
1960 On	Anglia (Incl. Station Wagons) Prefect Anglia ¼ ton Van Shop Manual (loose leaf) *	A5 AP5 ASV	A5M	1.25 1.25 1.25 6.00
1962-1966	Cortina and Cortina GT Shop Manual (loose leaf) *	C13	C13M	1.25 9.25
1967	Cortina and Cortina GT (Model C) Shop Manual	C670	C67M	1.25 5.50
1968	Cortina (1600 CC) Shop Manual	C680	C68M	1.25 7.75

YEAR	MODEL	OWNER'S MANUAL FORM NO.	SHOP MANUAL FORM NO.	PRICE EA.
1966 On	Cortina Lotus Shop Manual (loose leaf) *See binder below	CL1	CLM	\$1.25 3.25
1951-1956	Consul, Zephyr 6, Zodiac, MK I	Z1		1.25
1956-1962	Consul 375, Zephyr and Zodiac—MK II Shop Manual (bound)	Z2	Z1/2M	1.25 10.75
1962-1964	Consul 315 Shop Manual (loose leaf) *See binder below	C9	C9M	1.25 9.25
1962 On	Zephyr 4 and 6—MK III Zodiac—MK IV Shop Manual (loose leaf) *See binder below	Z3A Z3B	Z3M	1.25 1.25 9.25
1958-1961	Thames 800 Truck, Station Wagon and Bus Shop Manual (loose leaf) *See binder below	T8	T8M	1.25 5.75
FOR LOOSE LEAF SHOP MANUAL	Binder and Indexes	GS2 (bought with shop manual) (bought separately)		3.00 4.50
1967	Cortina Specifications	C67S		1.00

* See Listing FOR LOOSE LEAF SHOP MANUAL

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1957-1968 Ford/Mercury Service Publications

YEAR	DESCRIPTION	PRICE EA.
1957	Ford/Thunderbird Shop Manual	\$5.95
1958	Ford Car Shop Manual	5.95
1959	Ford Car Shop Manual	5.95
1960	Ford Car Shop Manual	5.95
1961	Ford Car Shop Manual	5.95
1962	Ford Car Shop Manual	5.95
1963	Ford Car Shop Manual Supplement (includes 1-7098-62 Ford Car Shop Manual)	5.95
1964	Ford/Mercury Shop Manual	5.95
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1967	Ford/Mercury Shop Manual (includes 1-7920C-67 Maintenance and Lubrication Manual)	5.95
1968	Ford/Mercury Shop Manual (includes 1-7920C-68 Maintenance and Lubrication Manual)	5.95
1960	Falcon Shop Manual	4.95
1961-62	Falcon Shop Manual	4.95
1963	Falcon Shop Manual Supplement (includes 1-7760-61-62 Falcon Shop Manual)	5.95
1964	Comet/Falcon Shop Manual	5.95
1965	Comet/Falcon/Fairlane/Mustang Shop Manual	5.95
1966	Comet/Falcon/Fairlane/Mustang Shop Manual	5.95
1967	Comet/Cougar/Falcon/Fairlane/Mustang Shop Manual (includes 1-7920C-67 Maintenance and Lubrication Manual)	5.95
1968	Cougar/Falcon/Fairlane/Montego/Mustang Shop Manual (includes 1-7920C-68 Maintenance and Lubrication Manual)	5.95
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1965	Thunderbird Shop Manual	5.95
1966	Thunderbird Shop Manual	5.95
1967	Thunderbird Shop Manual (includes 1-7920C-67 Maintenance and Lubrication Manual)	5.95

YEAR	DESCRIPTION	PRICE EA.
1968	Thunderbird Shop Manual Supplement (includes 1-7750-67 Thunderbird Shop Manual, and 1-7920C-68 Maintenance and Lubrication Manual)	\$6.95
1962	Fairlane Shop Manual	4.95
1963	Fairlane Shop Manual Supplement (includes 1-7780-62 Fairlane Shop Manual)	5.95
1964	Fairlane Shop Manual Supplement (includes 1-7780-62 and 1-7780-63 Fairlane Shop Manuals)	5.95
1961	Truck Shop Manual (Series 100 thru 800)	5.95
1962/63	Truck Shop Manual Supplement (Series 100 thru 800) (includes 1-7099A-61 Truck Shop Manual)	5.95
1964	Truck Shop Manual (Series 100-350)	5.95
1961	Truck Shop Manual (Series 850 thru 1100)	5.95
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1968	Bronco-Econoline Shop Manual (includes 1-7920T-68 Maintenance and Lubrication Manual)	6.95

1954-1968 Owner/Operator Manuals

YEAR	DESCRIPTION	PRICE EA.
1954/68	Ford	\$1.00
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1960 & 63/68	Falcon	1.00
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1965/67	High Performance	1.00

Miscellaneous Ford and Lincoln-Mercury Service Publications

YEAR	DESCRIPTION	PRICE EA.
	Dagenham Diesel Dorset Engine Manual	\$4.95
1967/68	Additional copies: Car Maintenance and Lubrication Manual— (specify model year)	1.75
1967/68	Additional copies: Truck Maintenance and Lubrication Manual— (specify model year)	2.00
	Model T	.75
	Model A	.75
1958	Lincoln and Mercury Turbo Drive Transmission Service Manual	1.00
1958	Transmatic (Ford)	1.00
1958	Air Suspension (Ford)	.75
1959/65	Ford Service Specifications (specify model year)	.75
1966/68	Ford Service Specifications—Car (specify model year)	.75
1966/68	Ford Service Specifications—Truck (specify model year)	.75

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ADDITIONAL SERVICE PUBLICATIONS

OTHER LINCOLN-MERCURY PUBLICATIONS

YEAR	DESCRIPTION	PRICE EA.
1957	Mercury Maintenance Manual	\$5.95
1958	Mercury Maintenance Manual	5.95
1959	Mercury Maintenance Manual	5.95
1960	Mercury Maintenance Manual	5.95
1961	Mercury Maintenance Manual	5.95
1962/63	Mercury Monterey Maintenance Manual Supplement (includes 1-6077-61 Mercury Maintenance Manual)	5.95
1960	Comet Maintenance Manual	4.95
1962/63	Comet Maintenance Manual Supplement (includes 1-10249-60 Comet Maintenance Manual)	5.95
1962	Meteor Maintenance Manual	4.95
1963	Meteor Maintenance Manual Supplement (includes 1-7194-62 Meteor Maintenance Manual)	5.95
1958	Lincoln and Continental Maintenance Manual	5.95
1959	Lincoln and Continental Maintenance Manual	5.95

YEAR	DESCRIPTION	PRICE EA.
1960	Lincoln and Continental Maintenance Manual	\$5.95
1964	Lincoln Continental Maintenance Manual	6.95
1965	Lincoln Continental Maintenance Manual	6.95
1966	Lincoln Continental Maintenance Manual	6.95
1967	Lincoln Continental Maintenance Manual (includes 1-7920C-67 Maintenance and Lubrication Manual)	6.95
1968	Lincoln Continental Maintenance Manual Supplement (includes 1-6076-67 Lincoln Continental Maintenance Manual and 1-7920C-68 Maintenance and Lubrication Manual)	7.95
1968	Continental Mark III Technical Data Shop Manual (includes 1-7920C-68 Maintenance and Lubrication Manual)	5.95
1958	Edsel Service Manual	5.95
1959	Edsel Maintenance Manual	5.95
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A TIGHT SEAL FOR VALVE COVER GASKETS ...

EVERYTIME!



INSTRUCTIONS FOR INSTALLING THE NEOPRENE RUBBER FORD VALVE COVER GASKETS

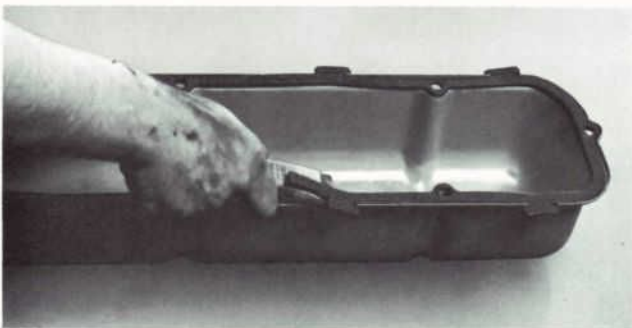
WHY A NEOPRENE RUBBER GASKET?

Since 1966, Ford has been using a Neoprene rubber valve cover gasket as replacement on all models, and for production on some medium and heavy trucks. This gasket is easier to handle, and has a longer shelf life than the cork type usually used on Ford vehicles. In addition, it has some better performance characteristics when used as a service replacement for cork.

When installing one of these gaskets in place of the production gasket, there are some steps that should be taken to assure a permanent seal. Observance of these installation instructions should help you avoid service comebacks on valve cover gaskets.

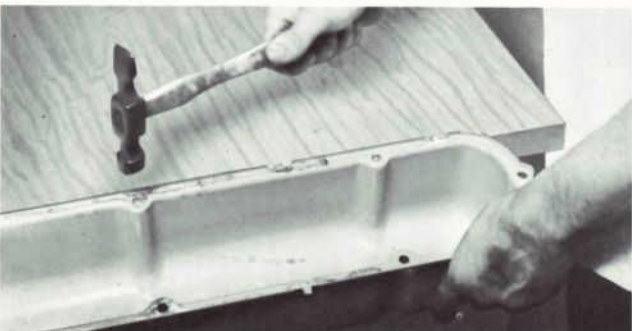
REMOVAL OF OLD GASKET

Remove the old cork gasket from the gasket surface of the valve cover, and from the machined surface of the cylinder head. Make sure to remove all particles from both surfaces so that the new gasket will not be deformed and thus cause possible leaks. Also remove all cement that may be found around the gasket sealing surfaces.



PREPARING THE SURFACES FOR THE NEW NEOPRENE RUBBER GASKET

The one thing that can prevent you from getting a good seal on the new valve cover gasket is a distorted mounting flange. This distortion is usually caused by the mounting bolts being torqued too tightly during installation. The original cork gasket will be deformed to more or less fit the bent cover, but another gasket cannot be satisfactorily installed to a cover in this condition.

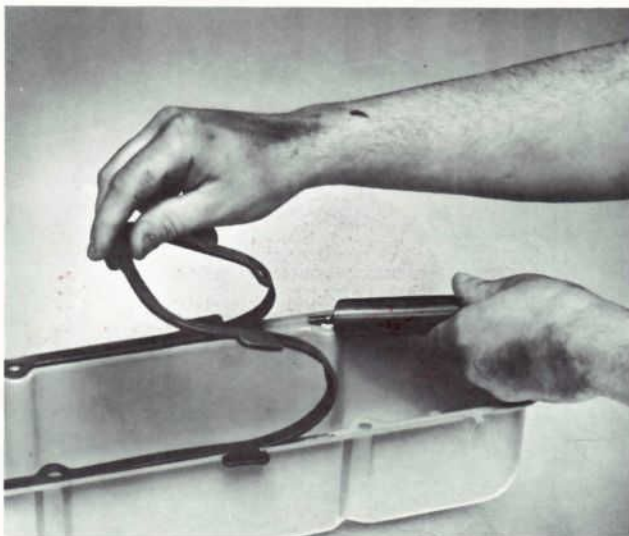


Using a block of wood and a hammer, straighten the flange until it shows true, using a straightedge.

Remove all oil from the gasket flange surface, and from the head surface, using an appropriate solvent. Failure to install the Neoprene rubber gasket to a dry surface may cause the gasket to "squeeze" out during torquing.

INSTALLING NEW GASKET

Sheet metal covers on some late models have built-in gasket retaining features such as cut-outs for gasket tabs. Others require the use of a good, firm-setting adhesive like Rotunda Perfect Seal Sealing Compound (R134-A) or Gasket and Seal Contact Adhesive (R112-A). Apply adhesive sparingly to the outer edge of the clean dry surface. Install gasket at the beginning of the job, so that the adhesive will have time to properly set before you reinstall the valve cover.



NOTE: Before reinstalling the valve cover, clean all oil return holes in the cylinder head. This will help the oil drain back into the crankcase, and will prevent build-up and undue pressure against the new valve cover gasket.

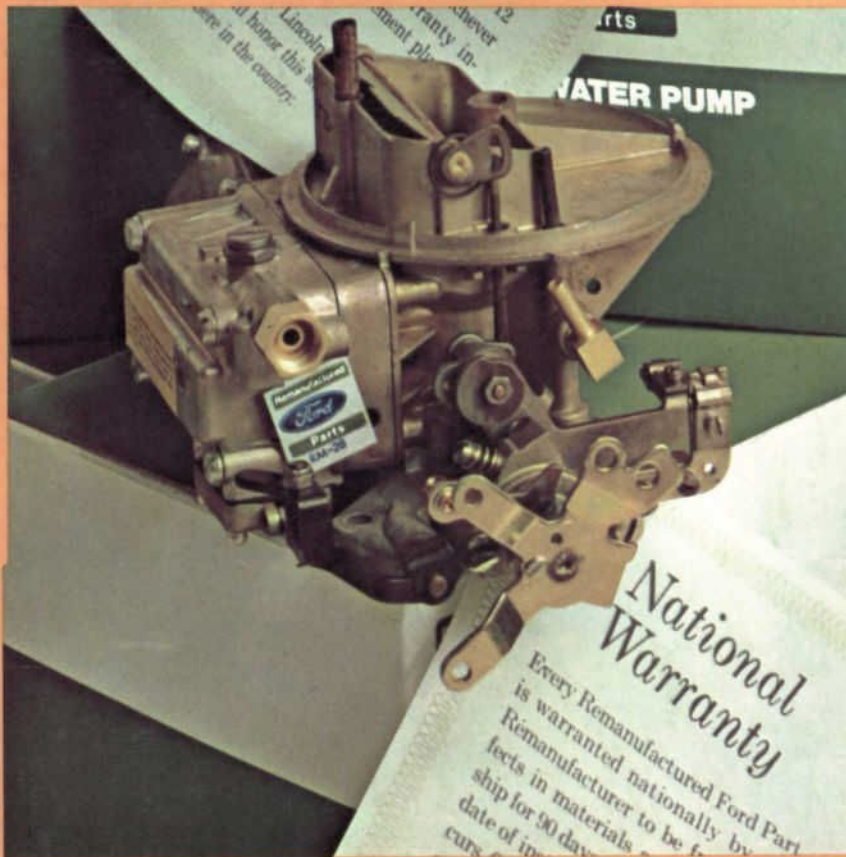
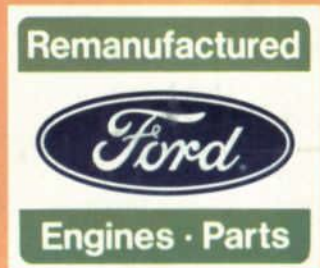
TIGHTENING VALVE COVER

Position the valve cover on the cylinder head, and check to see that all surfaces are properly aligned. Install the attaching bolts (and spacers or washers if required) and tighten slightly. Final-tighten the bolts in the recommended sequence to the following torque: 170-200-250-289-302 & 351 (3-5 ft.-lbs.); 390 & 428 (4-7 ft.-lbs.); 429 & 460 (5-6 ft.-lbs.); and 240 (7-9 ft.-lbs.).

Tightening beyond this limit will only distort the valve cover—thus producing uneven gasket pressures—and may even break the gasket. If you have followed all the instructions, you should have a gasket that will not leak, will protect the engine against dirt build-up, and will give you a happy customer.

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